

**BOROUGH OF FLEMINGTON
 MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)
 STATE JOB #6409314**

MAYOR AND COUNCIL MEMBERS

PHIL GREINER, MAYOR

BRIAN SWINGLE, COUNCIL PRESIDENT
 MARC HAIN, COUNCIL VICE PRESIDENT

JOHN GORMAN, COUNCIL MEMBER
 BROOKE LIEBOWITZ, COUNCIL MEMBER
 MICHELLE OBERST, COUNCIL MEMBER
 AL BREWER, COUNCIL MEMBER

BOROUGH ENGINEER

ROBERT MARTUCCI, P.E., C.M.E.

UTILITY OWNERS LIST:

TELEPHONE: (TEL.# 908-735-3334)
 EMBARQ
 160 CENTER STREET, P.O. BOX 4002
 CLINTON, NJ 08809
 CONTACT: DONNA CIOPPI

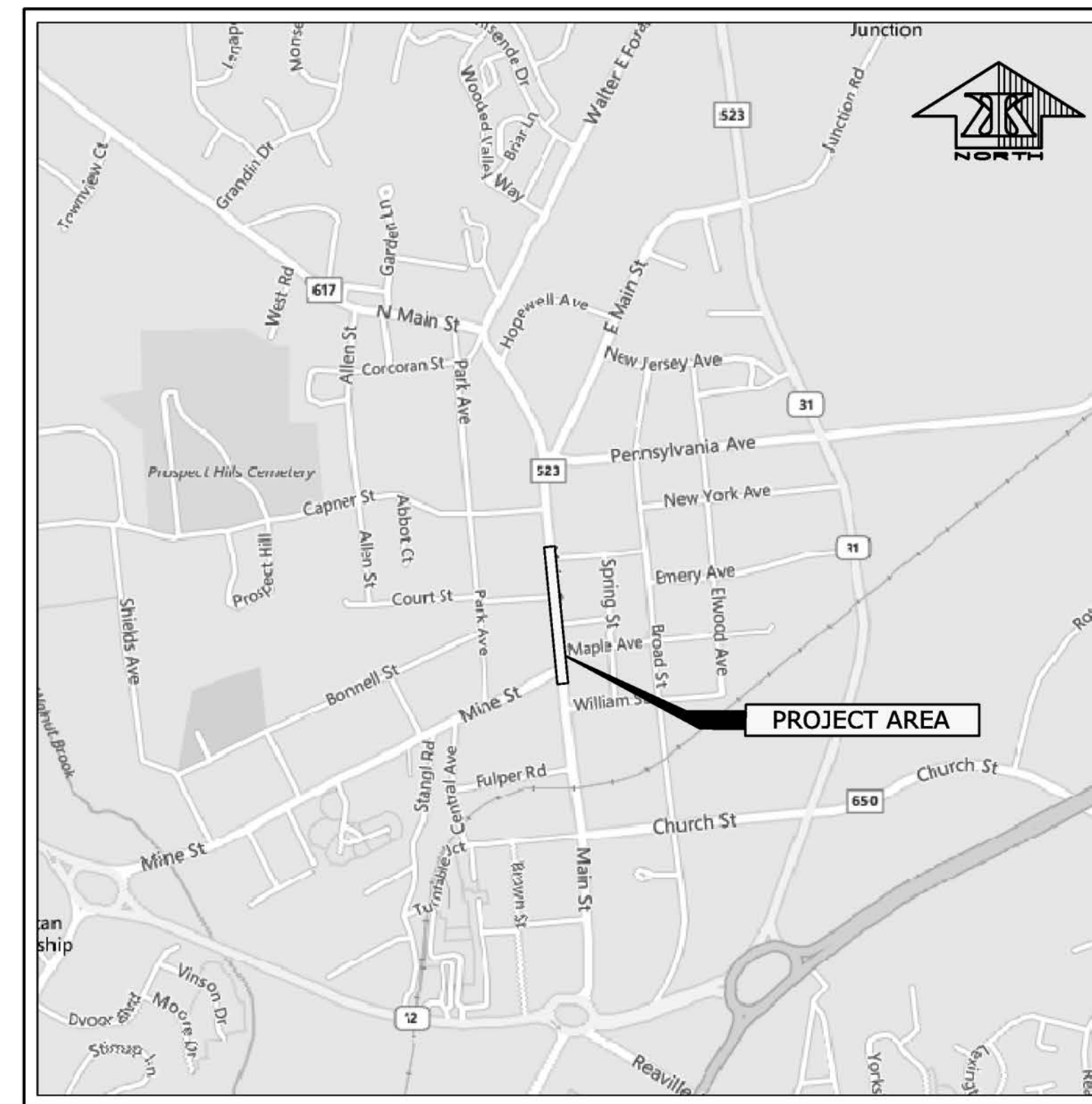
WATER: (TEL.# 908-782-8840)
 BOROUGH OF FLEMINGTON WATER DEPT.
 38 PARK AVENUE
 FLEMINGTON, NJ 08822
 CONTACT: REBECCA NEWMAN, UTILITIES CLERK

GAS: (TEL.# 908-662-8321)
 ELIZABETHTOWN GAS COMPANY
 520 GREEN LANE
 UNION, NJ 07083
 CONTACT: GREG BALINT

CABLE: (TEL.# 732-652-2775)
 COMCAST CABLE
 100 RANDOLPH ROAD
 SOMERSET, NJ 08873
 CONTACT: SAL DIMAGGIO

ELECTRIC: (TEL.# 973-401-8521)
 JERSEY CENTRAL POWER & LIGHT (JCP&L)
 300 MADISON AVENUE
 MORRISTOWN, NJ 07962-0911
 CONTACT: FRANK MERCADANTE

SEWER: (TEL.# 908-782-8840)
 BOROUGH OF FLEMINGTON SEWER DEPT.
 38 PARK AVENUE
 FLEMINGTON, NJ 08822
 CONTACT: REBECCA NEWMAN, UTILITIES CLERK



KEY MAP
 1"=1000'

INDEX OF SHEETS	
SHEET	DESCRIPTION
	COVER SHEET WITH KEY MAP
2	GENERAL NOTES, LEGEND & ESTIMATE OF QUANTITIES
3	TYPICAL SECTIONS
4-5	EXISTING CONDITIONS PLAN
6-7	SITE PREPARATION PLAN
8-9	LAYOUT & DIMENSIONING PLAN
10-11	GRADING & UTILITY PLAN
12-13	ACCESSIBLE CURB RAMP DETAILS
14	ELECTRICAL SYMBOLS, LEGEND & DETAILS
15-16	PARTIAL ELECTRICAL SITE PLAN
17-18	PLANTING PLAN
19-20	SIGNING & STRIPING PLAN
21-22	SOIL EROSION & SEDIMENT CONTROL PLAN
23-24	PHASING PLAN
25-28	CONSTRUCTION DETAILS
29-31	TRAFFIC CONTROL PLAN

*NEW JERSEY DEPARTMENT OF TRANSPORTATION STANDARD
 SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2007 AS
 AMENDED BY THE SUPPLEMENTARY SPECIFICATIONS FOR FEDERAL AID
 PROJECTS, EDITION 2011, TO GOVERN UNLESS OTHERWISE INDICATED IN
 THESE PLANS OR THE PROJECT SPECIFICATIONS AND IN ACCORDANCE
 WITH CURRENT NJDOT STANDARD CONSTRUCTION DETAILS*

ENGINEER:

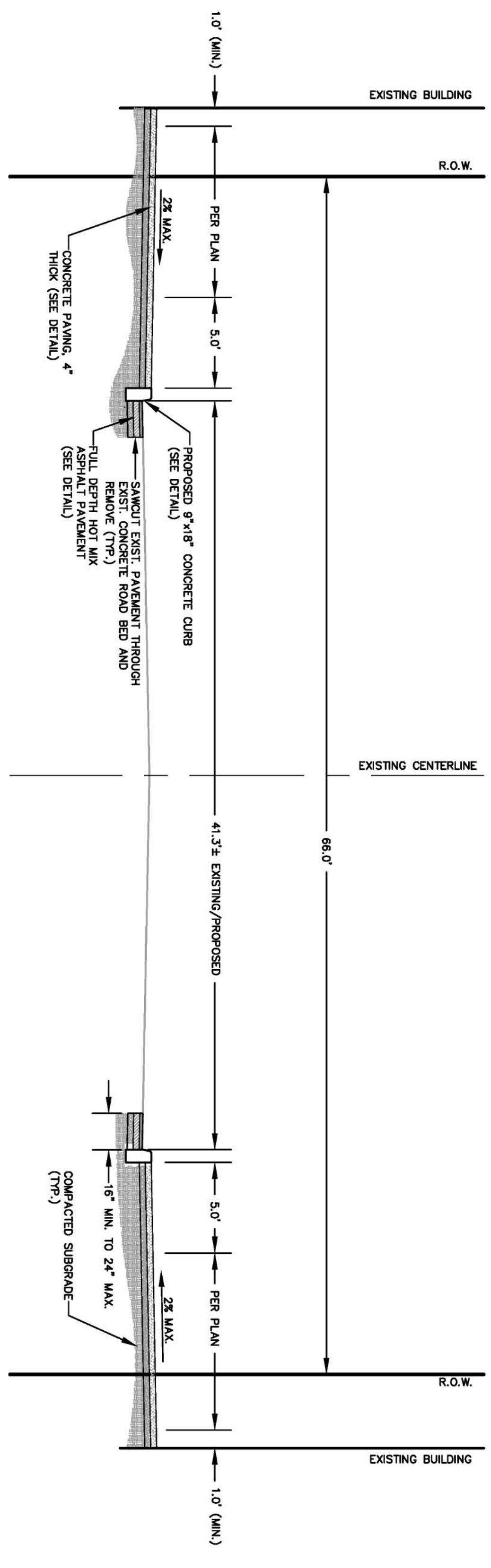


Keller & Kirkpatrick

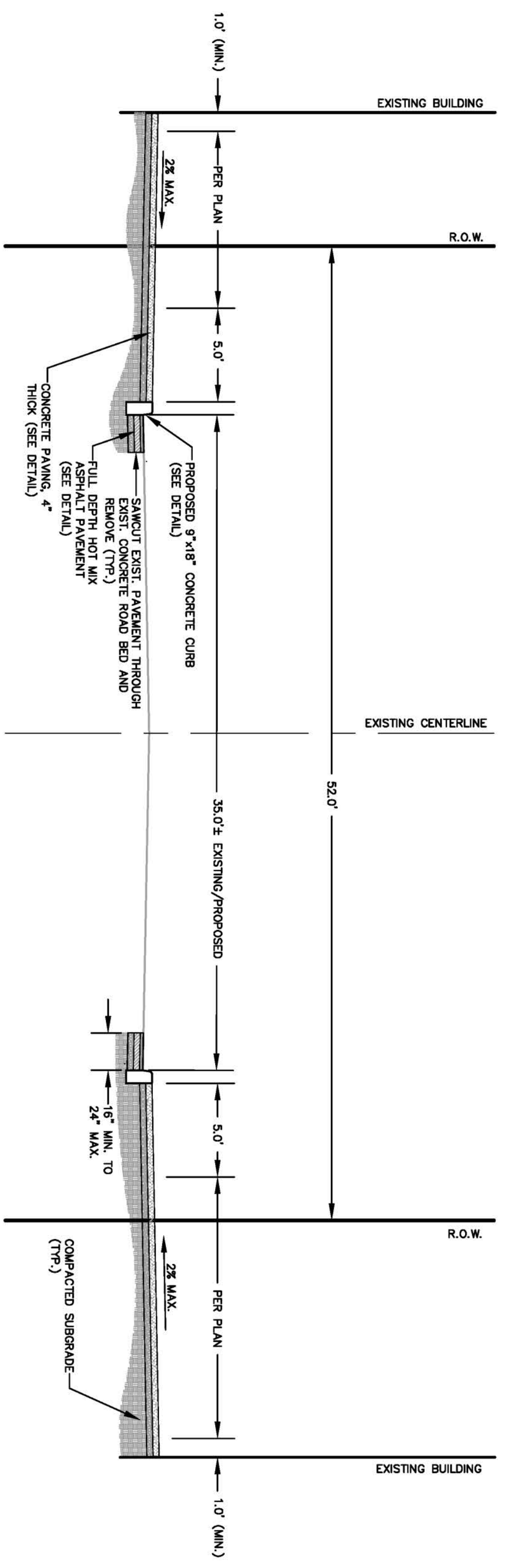
301 Gibraltar Drive, Suite 2A, Morris Plains, NJ, 07960 973-377-8500
 Certificate of Authorization #24GA28037400

ANDREW CANGIANO, Professional Engineer, Lic. 43111

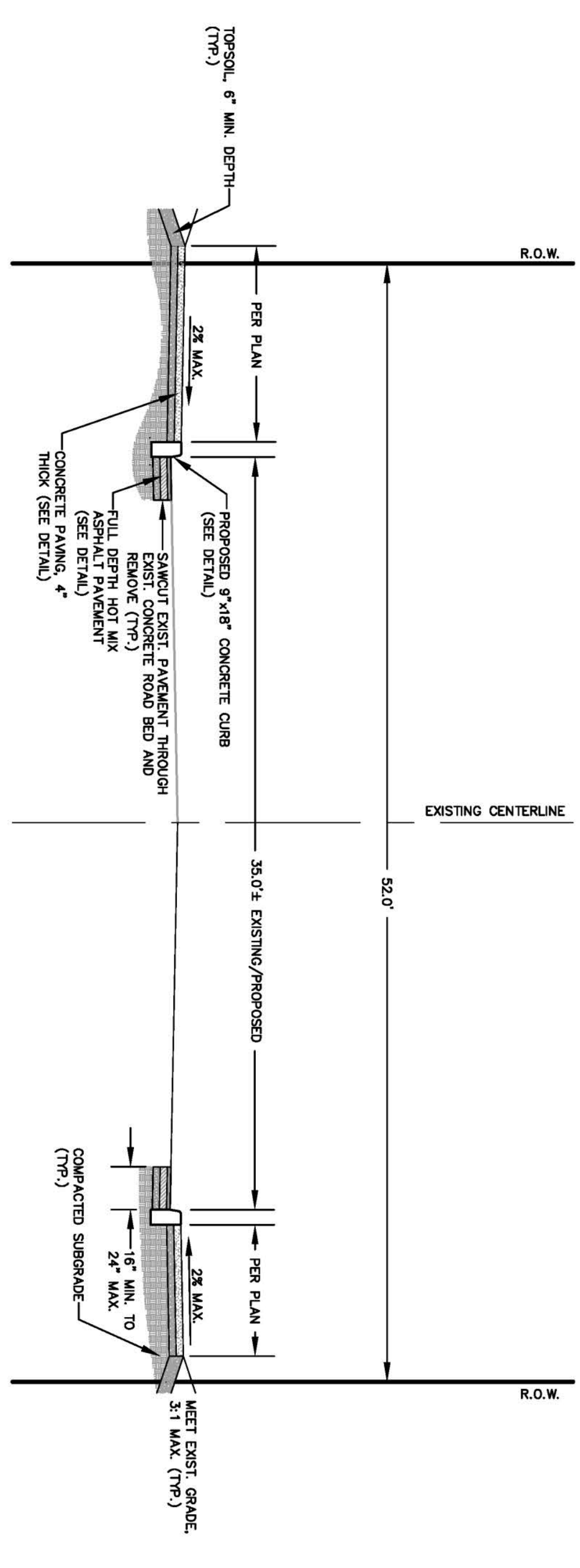
**APRIL 2017
 REVISED THROUGH JUNE 2018**



MAIN STREET
TYPICAL SECTION 'A'



MAIN STREET
TYPICAL SECTION 'B'



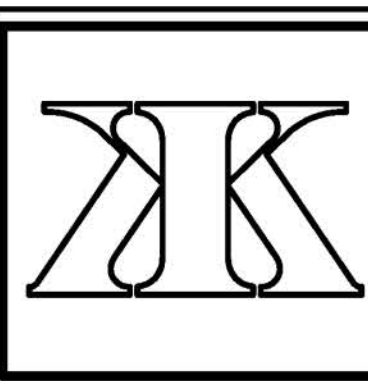
MAIN STREET
TYPICAL SECTION 'C'

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3	NO REVISION THIS SHEET	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18
1	PER NJDOT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17 SCALE: none FLD. BK.:
 PROJ.: 2016020 CHKD.: PAGE:

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Andrew Cangiano
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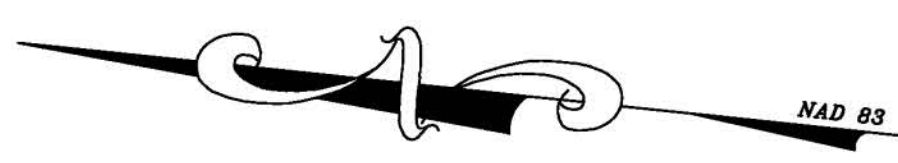
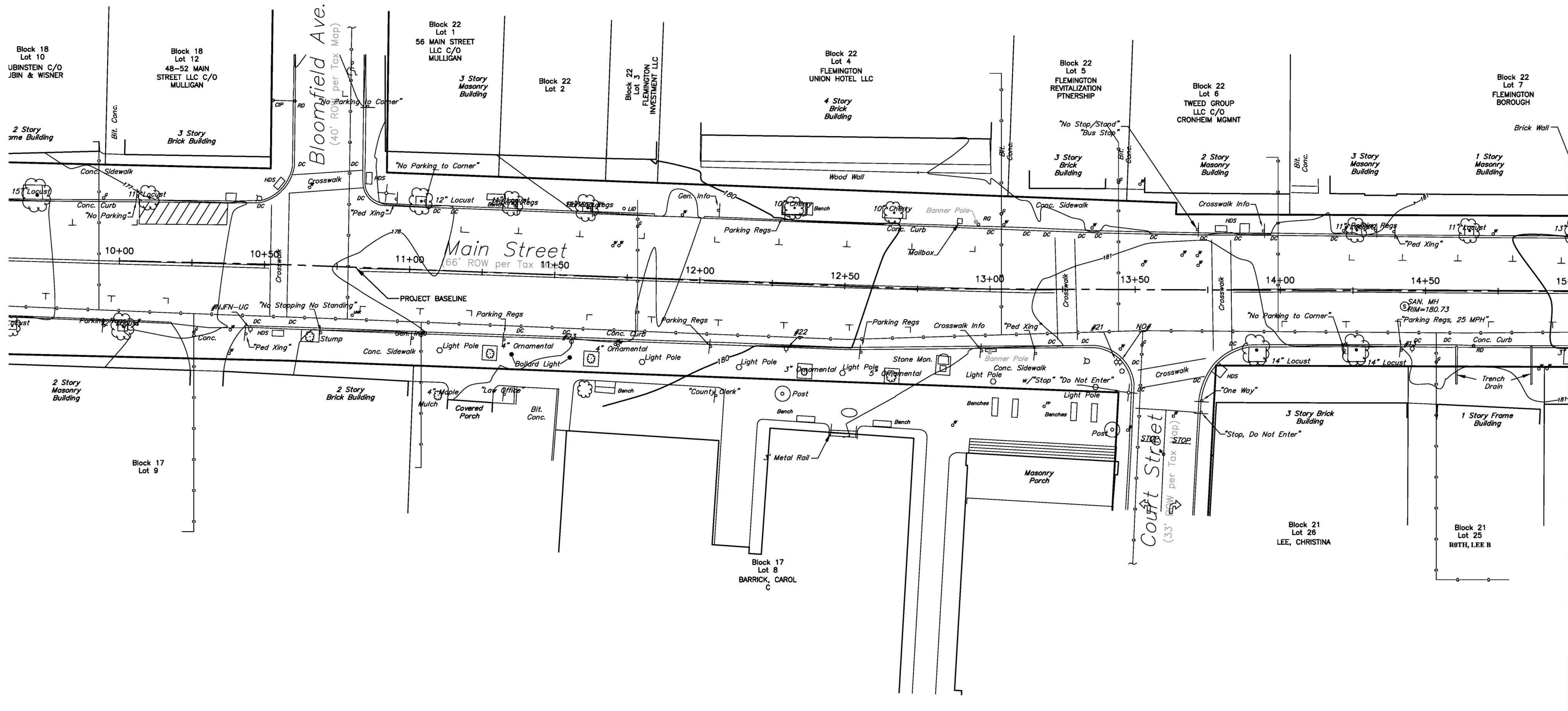
MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET

TYPICAL SECTIONS

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No. **3**
 OF **31**

SEE SHEET 2 OF 31 FOR GENERAL NOTES

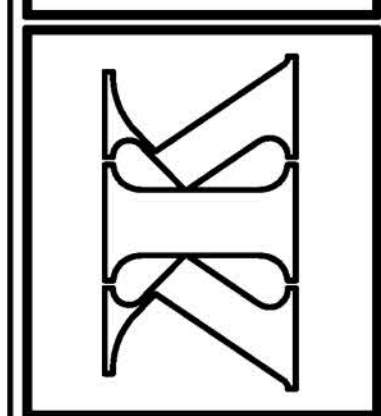


MATCHLINE STATION 15+00 - SEE SHEET 5 OF 31

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3	NO REVISION THIS SHEET	5/16/18
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1	PER MDDOT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17
 SCALE: 1" = 20'
 FLD. BK.: 75-6
 PROJ.: 2016020
 CHKD.:
 PAGE: 28

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MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
EXISTING CONDITIONS PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
4
 OF **31**

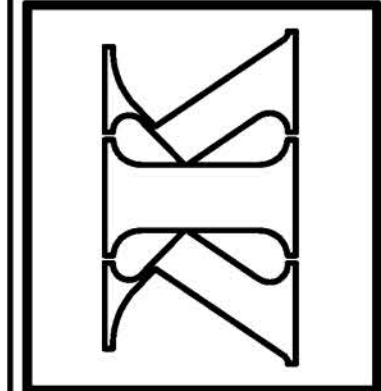
SEE SHEET 2 OF 31 FOR GENERAL NOTES

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2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18	
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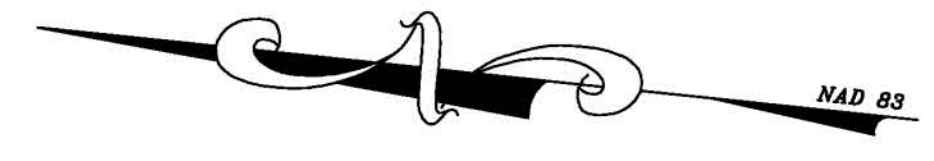
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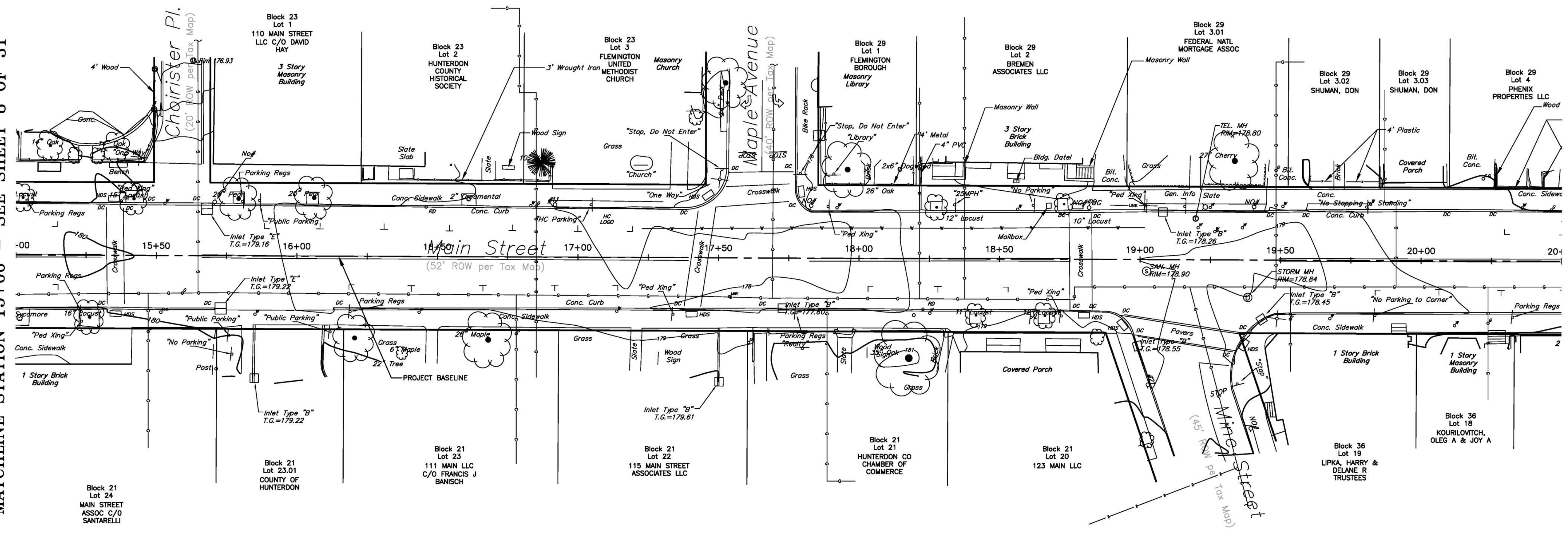
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 EXISTING CONDITIONS PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
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SHEET No.
 5
 OF 31

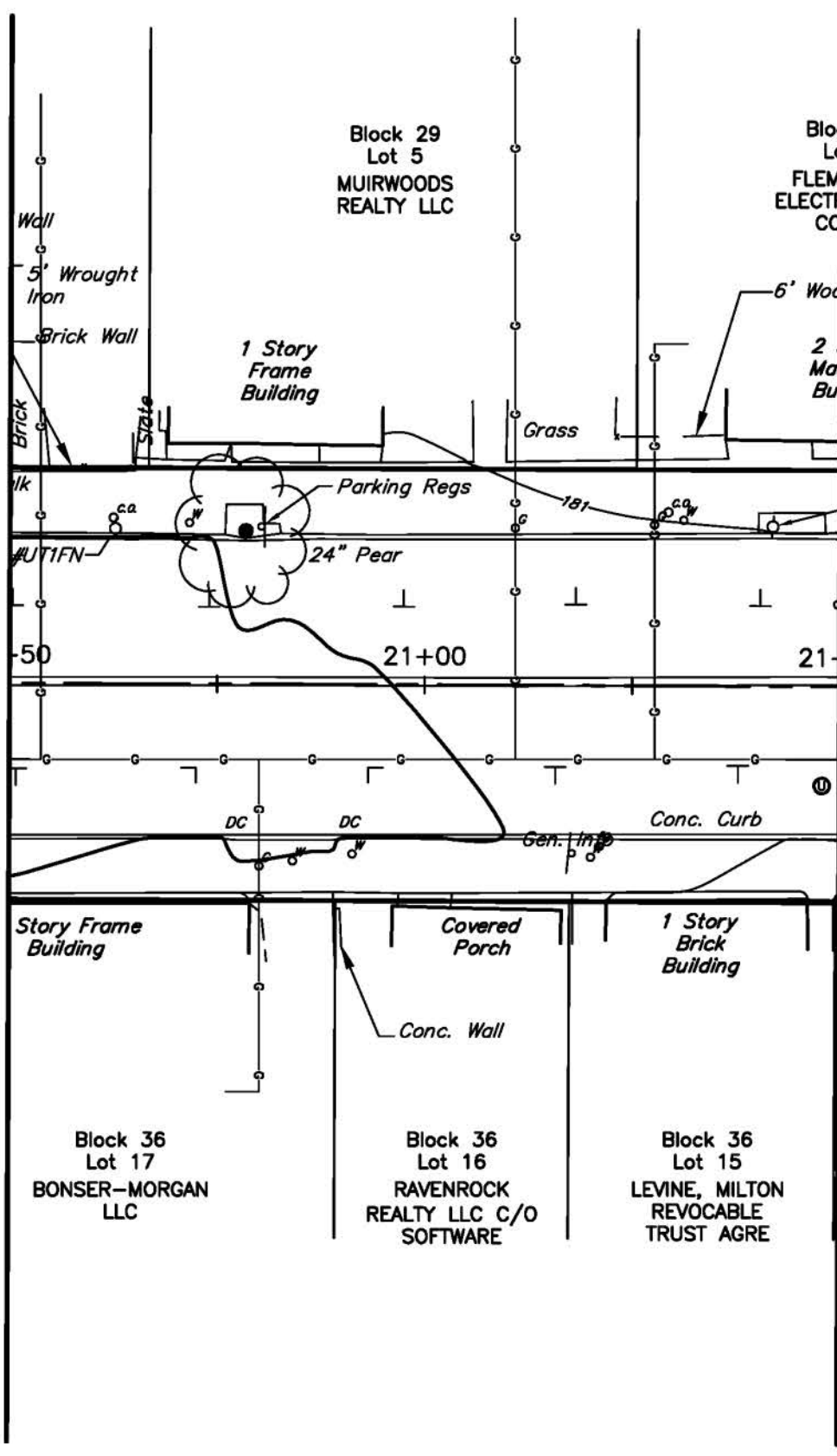


MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31

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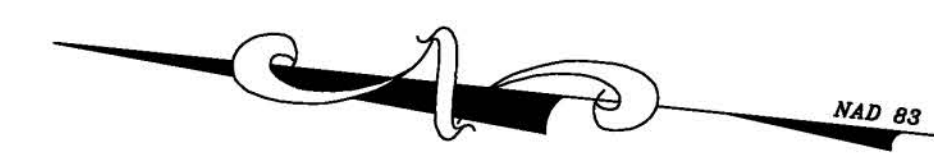
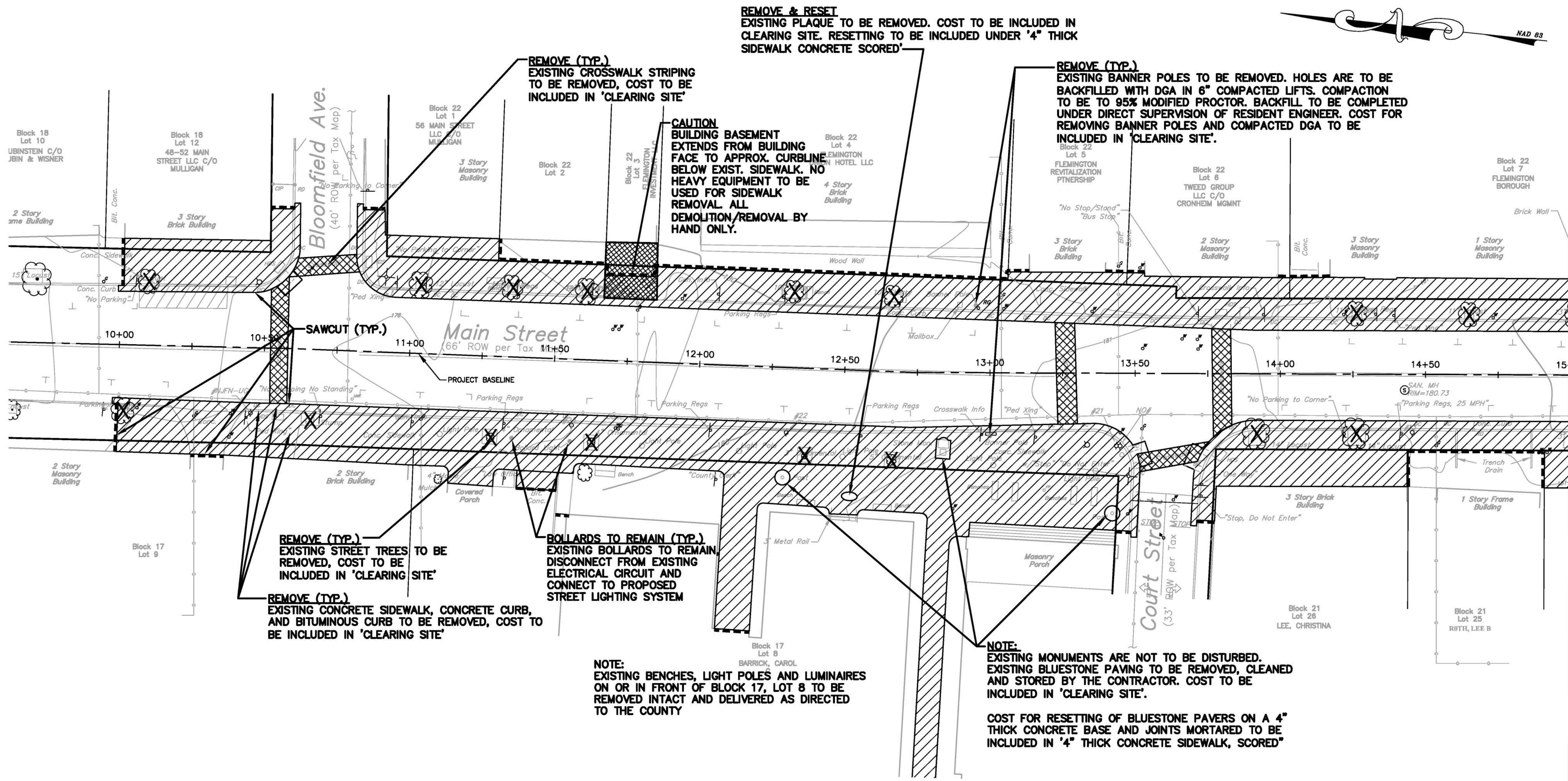


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SEE SHEET 2 OF 31 FOR GENERAL NOTES

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MATCHLINE STATION 15+00 - SEE SHEET 7 OF 31

SITE PREPARATION NOTES:

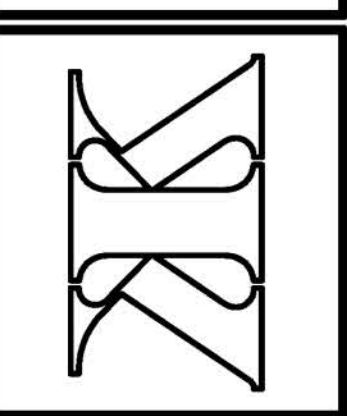
1. ALL REGULATORY AND GUIDE SIGNS & POSTS WITHIN THE PROJECT LIMITS TO BE REPLACED.
2. PAYMENT UNDER THE PAYMENT ITEM, REGULATORY AND GUIDE SIGNS, INCLUDES THE COST OF REMOVAL OF THE EXISTING SIGN AND POST, INSTALLING THE NEW POST AND FASTENING THE NEW SIGN ON THE NEW POST (SEE SIGNING AND STRIPING PLAN).
3. THE NEW POSTS ARE TO BE INSTALLED EITHER AT THE SAME LOCATION OR NEW LOCATION AS SHOWN ON THE SIGNING AND STRIPING PLAN AND/OR AS DIRECTED BY THE ENGINEER.
4. ALL MAILBOXES, PAPER BOXES, SITE FURNISHINGS THAT ARE NOT TO BE REMOVED FROM SITE ARE TO BE STORED DURING CONSTRUCTION AND RESET AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN CLEARING SITE.

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4	NO REVISION THIS SHEET	3/28/18
3	NO REVISION THIS SHEET	10/17/17
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1	PER MADD COMMENT RE: ROW CERTIFICATION	

DATE: 4/24/17	SCALE: 1" = 20'	F.L.D. BK.: 75-6
PROJ.: 2016020	CHKD.:	PAGE: 28

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 PLANNERS

Andrew Cangiano
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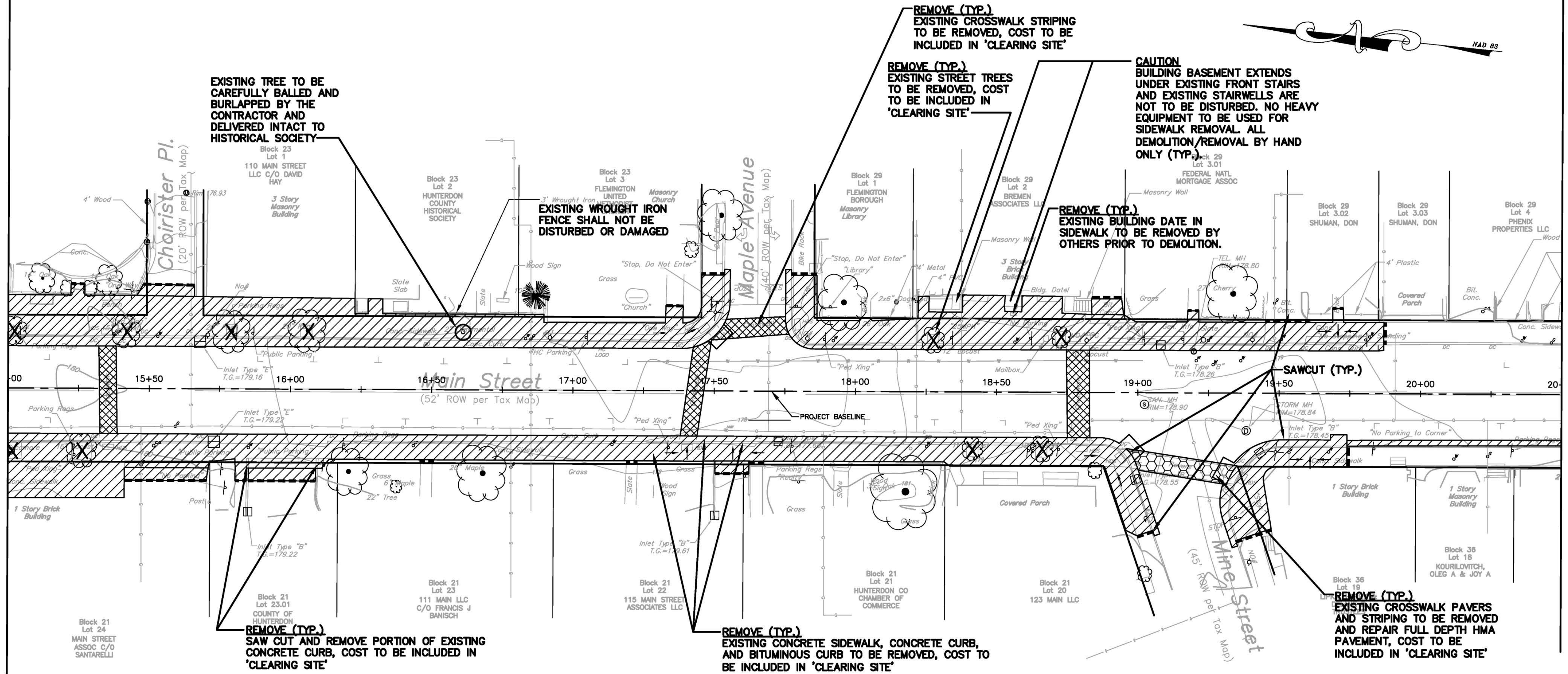
MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
SITE PREPARATION PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.	6
OF	31

SEE SHEET 2 OF 31 FOR GENERAL NOTES

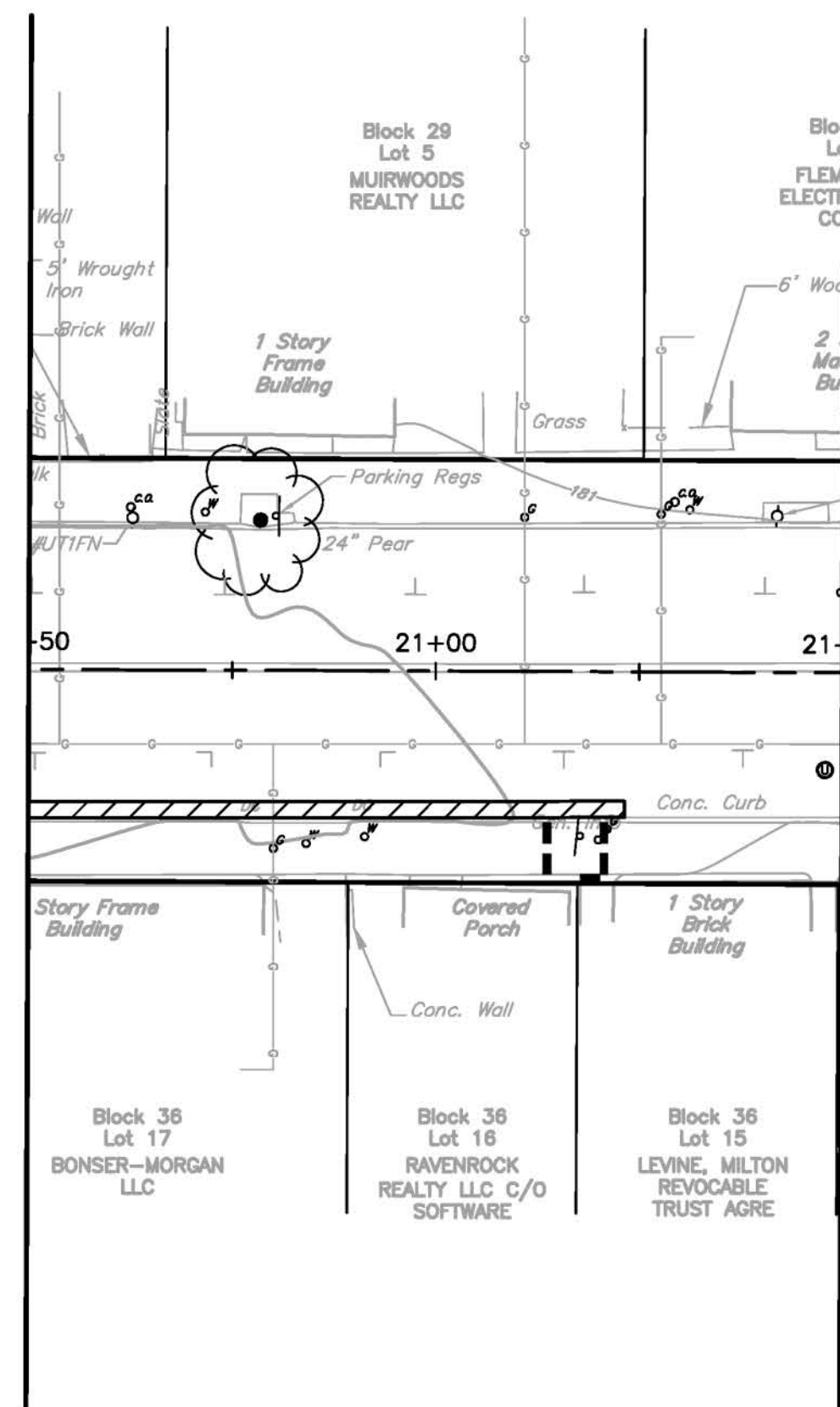
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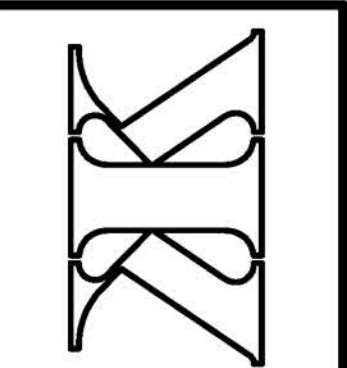
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DATE: 4/24/17 SCALE: 1" = 20' FLD. BK.: 75-6
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SITE PREPARATION PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
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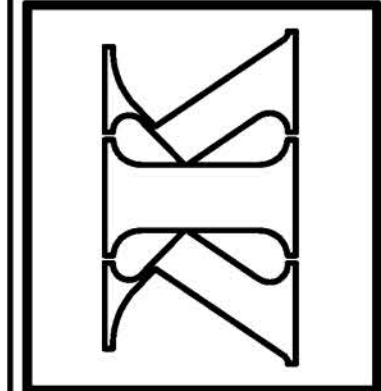
SHEET No. 31 OF 31

SEE SHEET 2 OF 31 FOR GENERAL NOTES

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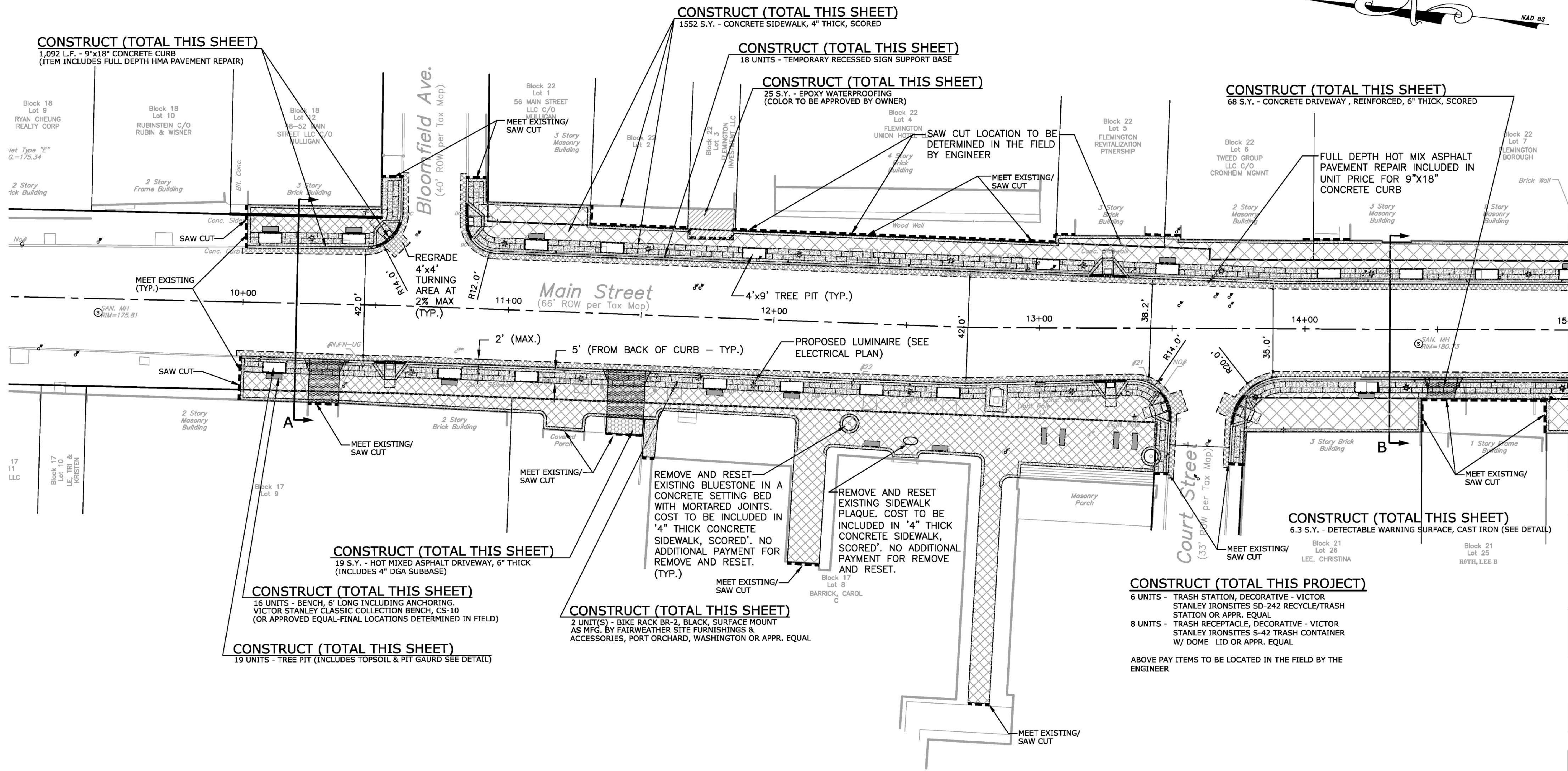
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BLOOMFIELD AVENUE TO MINE STREET
LAYOUT & DIMENSIONING PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
8
 OF **31**



CONSTRUCT (TOTAL THIS SHEET)
 1,092 L.F. - 9"x18" CONCRETE CURB
 (ITEM INCLUDES FULL DEPTH HMA PAVEMENT REPAIR)

CONSTRUCT (TOTAL THIS SHEET)
 1552 S.Y. - CONCRETE SIDEWALK, 4" THICK, SCORED

CONSTRUCT (TOTAL THIS SHEET)
 18 UNITS - TEMPORARY RECESSED SIGN SUPPORT BASE

CONSTRUCT (TOTAL THIS SHEET)
 25 S.Y. - EPOXY WATERPROOFING
 (COLOR TO BE APPROVED BY OWNER)

CONSTRUCT (TOTAL THIS SHEET)
 68 S.Y. - CONCRETE DRIVEWAY, REINFORCED, 6" THICK, SCORED

FULL DEPTH HOT MIX ASPHALT
 PAVEMENT REPAIR INCLUDED IN
 UNIT PRICE FOR 9"x18"
 CONCRETE CURB

CONSTRUCT (TOTAL THIS SHEET)
 19 S.Y. - HOT MIXED ASPHALT DRIVEWAY, 6" THICK
 (INCLUDES 4" DGA SUBBASE)

CONSTRUCT (TOTAL THIS SHEET)
 16 UNITS - BENCH, 6' LONG INCLUDING ANCHORING.
 VICTOR STANLEY CLASSIC COLLECTION BENCH, CS-10
 (OR APPROVED EQUAL-FINAL LOCATIONS DETERMINED IN FIELD)

CONSTRUCT (TOTAL THIS SHEET)
 19 UNITS - TREE PIT (INCLUDES TOPSOIL & PIT GAURD SEE DETAIL)

CONSTRUCT (TOTAL THIS SHEET)
 2 UNIT(S) - BIKE RACK BR-2, BLACK, SURFACE MOUNT
 AS MFG. BY FAIRWEATHER SITE FURNISHINGS &
 ACCESSORIES, PORT ORCHARD, WASHINGTON OR APPR. EQUAL

CONSTRUCT (TOTAL THIS PROJECT)
 6 UNITS - TRASH STATION, DECORATIVE - VICTOR
 STANLEY IRONSITES SD-242 RECYCLE/TRASH
 STATION OR APPR. EQUAL
 8 UNITS - TRASH RECEPTACLE, DECORATIVE - VICTOR
 STANLEY IRONSITES S-42 TRASH CONTAINER
 W/ DOME LID OR APPR. EQUAL

ABOVE PAY ITEMS TO BE LOCATED IN THE FIELD BY THE
 ENGINEER

REMOVE AND RESET
 EXISTING BLUESTONE IN A
 CONCRETE SETTING BED
 WITH MORTARED JOINTS.
 COST TO BE INCLUDED IN
 '4" THICK CONCRETE
 SIDEWALK, SCORED'. NO
 ADDITIONAL PAYMENT FOR
 REMOVE AND RESET.
 (TYP.)

REMOVE AND RESET
 EXISTING SIDEWALK
 PLAQUE. COST TO BE
 INCLUDED IN '4" THICK
 CONCRETE SIDEWALK,
 SCORED'. NO ADDITIONAL
 PAYMENT FOR REMOVE
 AND RESET.

CONSTRUCT (TOTAL THIS SHEET)
 6.3 S.Y. - DETECTABLE WARNING SURFACE, CAST IRON (SEE DETAIL)

Block 21
 Lot 26
 LEE, CHRISTINA
 Block 21
 Lot 25
 ROTH, LEE B

TABLE OF QUANTITIES

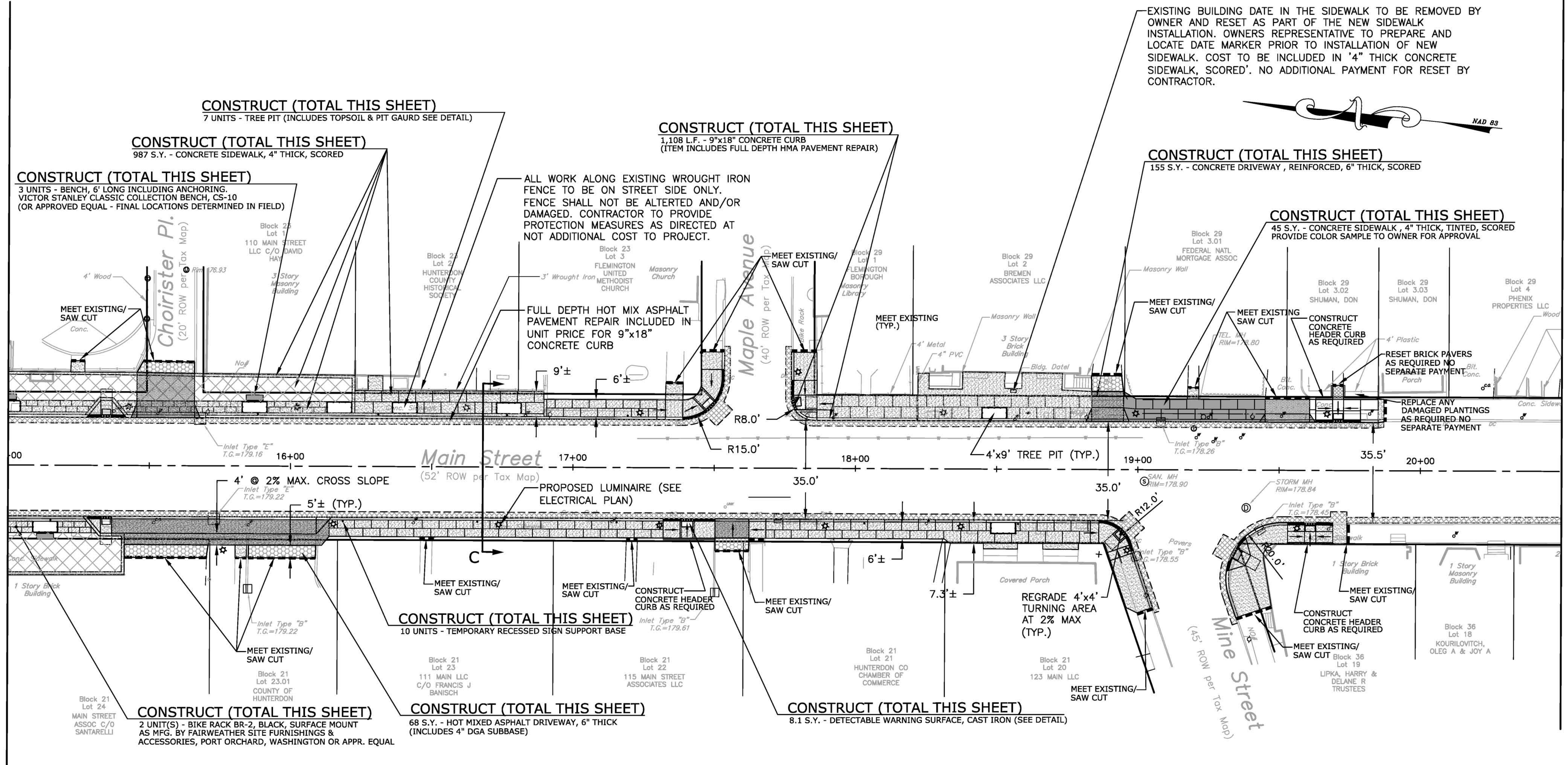
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19	CONCRETE SIDEWALK, 4" THICK	SY	1552
21	CONCRETE DRIVEWAY, REINFORCED, 6" THICK	SY	68
22	HOT MIX ASPHALT DRIVEWAY, 6" THICK	SY	19
23	DETECTABLE WARNING SURFACE, CAST IRON	SY	6.3
24	9"x18" CONCRETE VERTICAL CURB	LF	1092
29	TEMPORARY RECESSED SIGN SUPPORT BASE	UNITS	18
32	BENCHES - (6)	UNITS	16
33	BIKE RACK	UNITS	2
45	TREE PITS (INCLUDES TOPSOIL & PIT GAURD)	UNITS	19
49	EPOXY WATERPROOFING	SY	25

SEE SHEET 2 OF 31 FOR GENERAL NOTES

MATCHLINE STATION 15+00 - SEE SHEET 9 OF 31

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MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31



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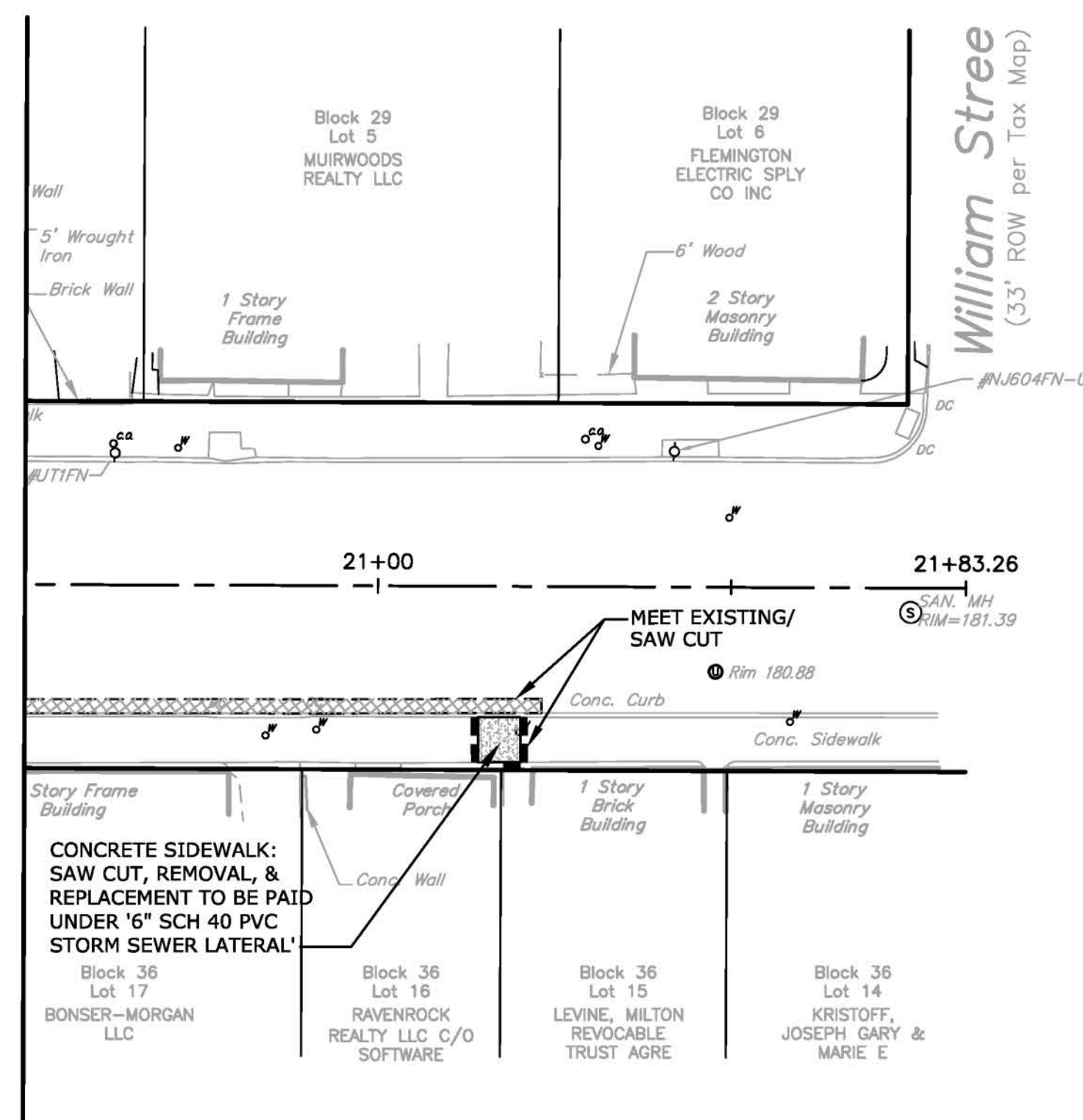


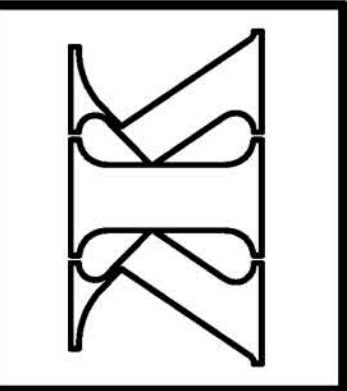
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21	CONCRETE DRIVEWAY, REINFORCED, 6" THICK	SY	155
22	HOT MIX ASPHALT DRIVEWAY, 6" THICK	SY	68
23	DETECTABLE WARNING SURFACE, CAST IRON	SY	8.1
24	9"x18" CONCRETE VERTICAL CURB	LF	1108
29	TEMPORARY RECESSED SIGN SUPPORT BASE	UNITS	10
32	BENCHES - (6)	UNITS	3
33	BIKE RACK	UNITS	2
45	TREE PITS (INCLUDES TOPSOIL & PIT GUARD)	UNITS	7

SEE SHEET 2 OF 31 FOR GENERAL NOTES

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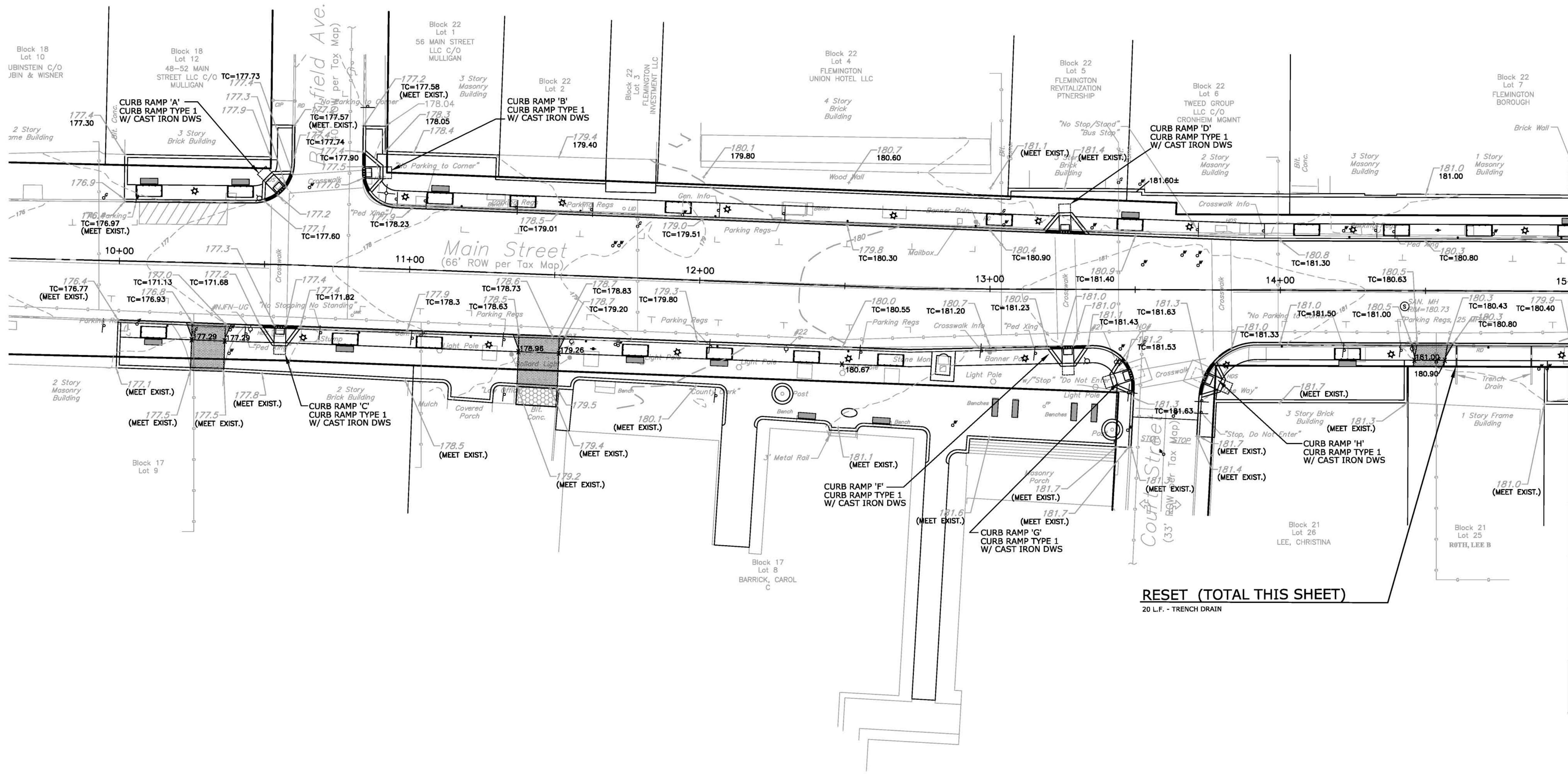
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MAIN STREET SIDEWALK & STREETScape
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LAYOUT & DIMENSIONING PLAN
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 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
9
 OF **31**



MATCHLINE STATION 15+00 - SEE SHEET 11 OF 31

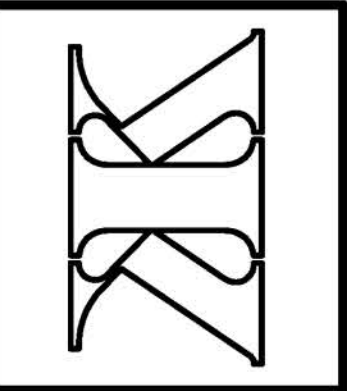
TABLE OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
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SEE SHEET 2 OF 31 FOR GENERAL NOTES

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MAIN STREET SIDEWALK & STREETSCAPE
 BLOOMFIELD AVENUE TO MINE STREET
GRADING & UTILITY PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

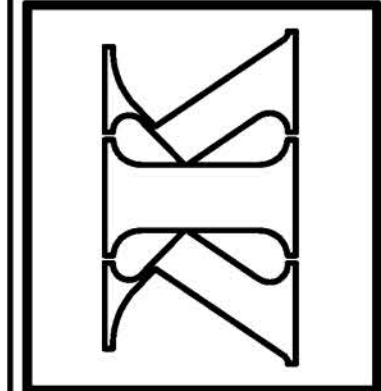
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10
 OF **31**

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1	PER MDDT COMMENT RE: ROW CERTIFICATION	10/17/17	

DATE: 4/24/17
 SCALE: 1" = 20'
 FLD. BK.: 75-6
 PROJ.: 2016020
 PAGE: 28
 CHKD.:
 REVISIONS

Keller & Kirkpatrick Inc.
 501 Gibraltar Drive, Suite 204, Morris Plains, NJ 07950 973-377-8600
 CONSULTING ENGINEERS
 LAND SURVEYORS
 LANDSCAPE ARCHITECTS
 PLANNERS
 ANDREW CANGIANO, Professional Engineer, Lic. #911

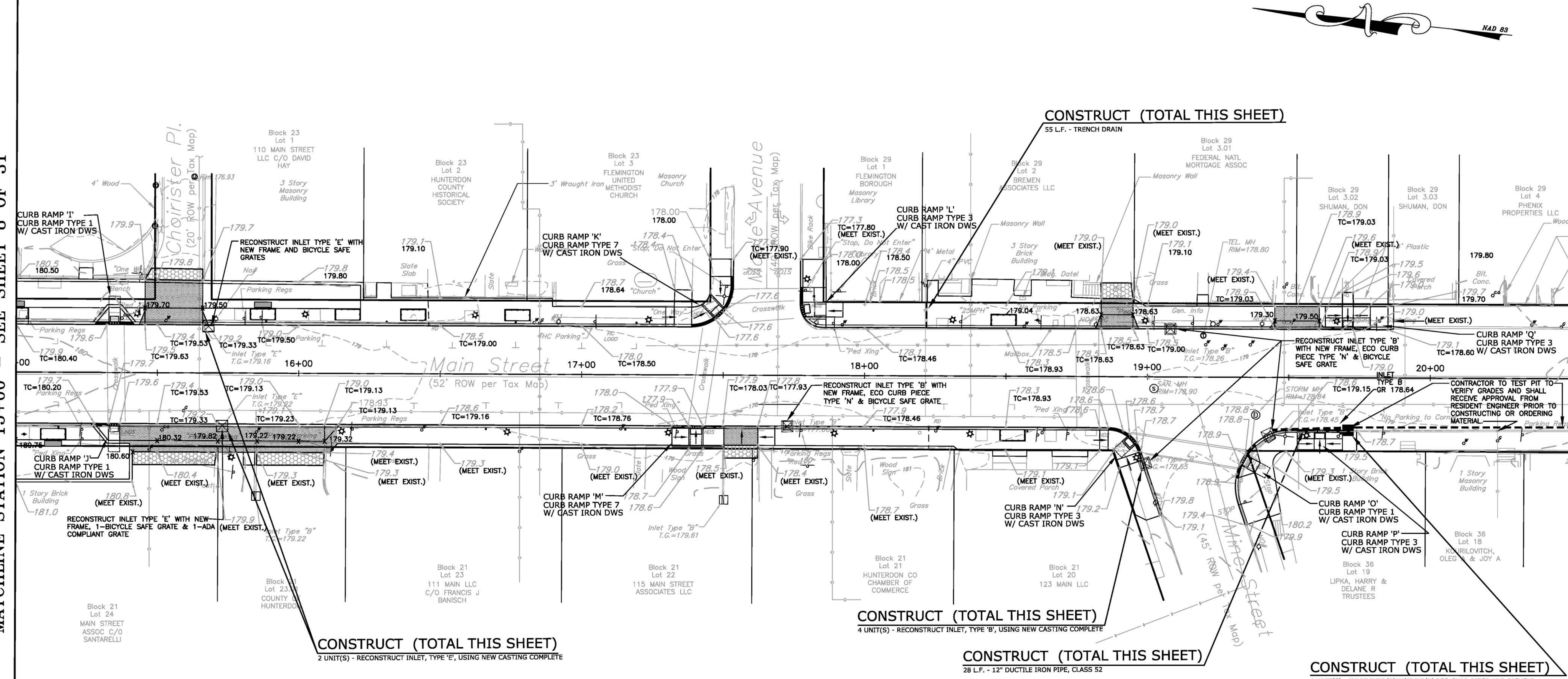


MAIN STREET SIDEWALK & STREETSCAPE
 BLOOMFIELD AVENUE TO MINE STREET
GRADING & UTILITY PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

SHEET No.
11
 OF
31

MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31

MATCHLINE STATION 20+50 - THIS SHEET



MATCHLINE STATION 20+50 - THIS SHEET

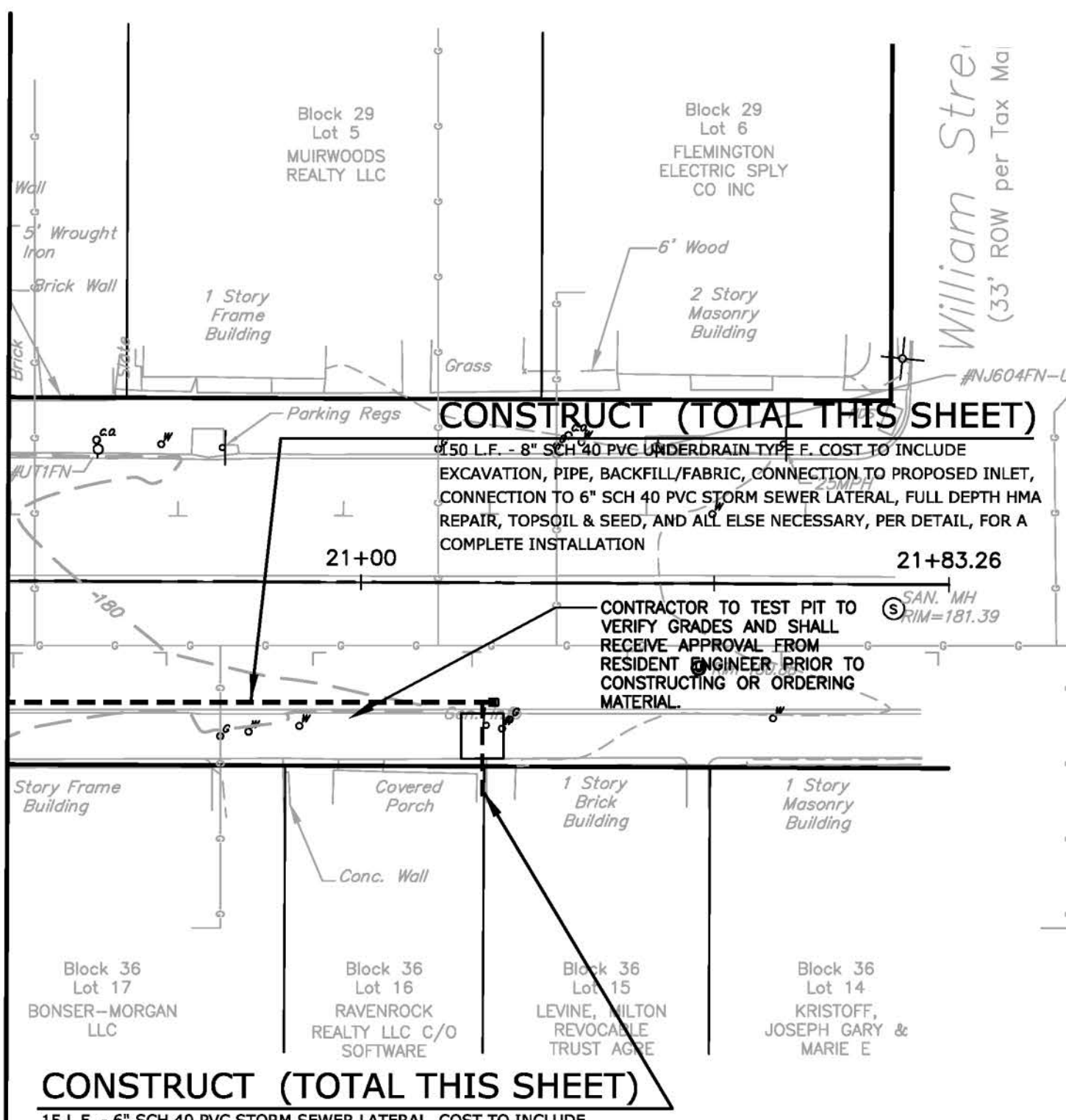
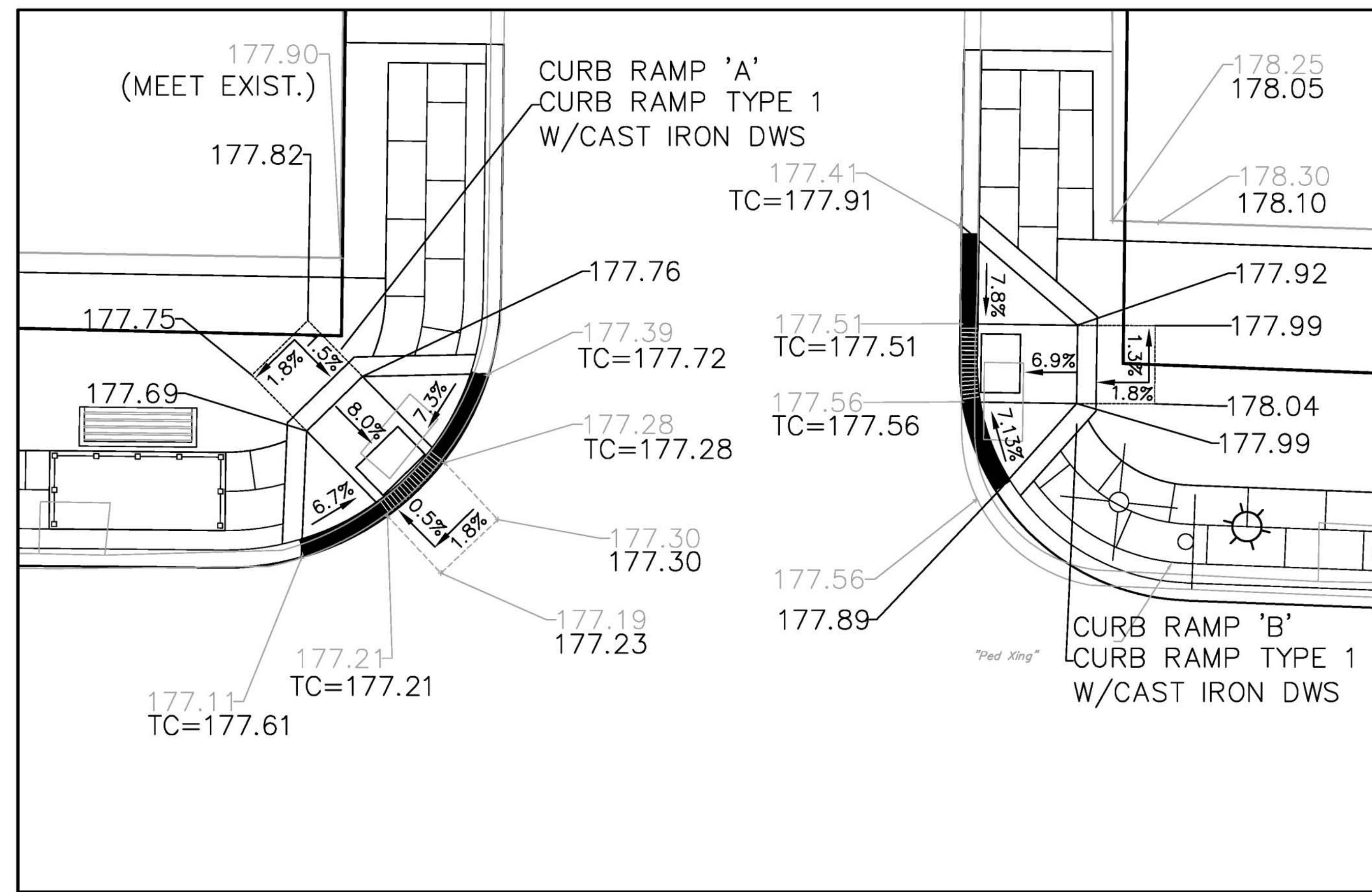


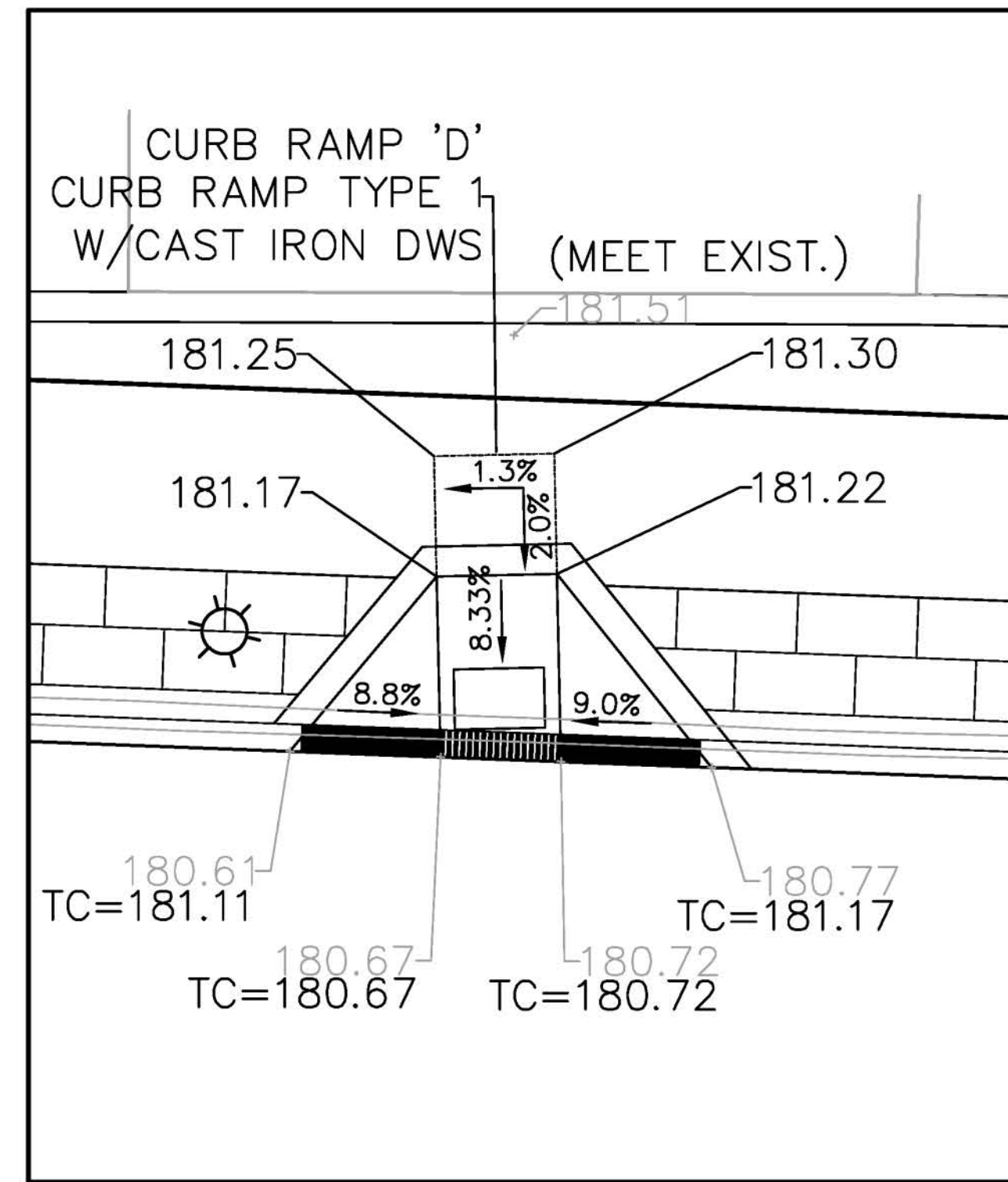
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ITEM NO.	DESCRIPTION	UNIT	QUANTITY SHEET 11
14	TRENCH DRAIN	LF	55
15	12\"/>		

SEE SHEET 2 OF 31 FOR GENERAL NOTES

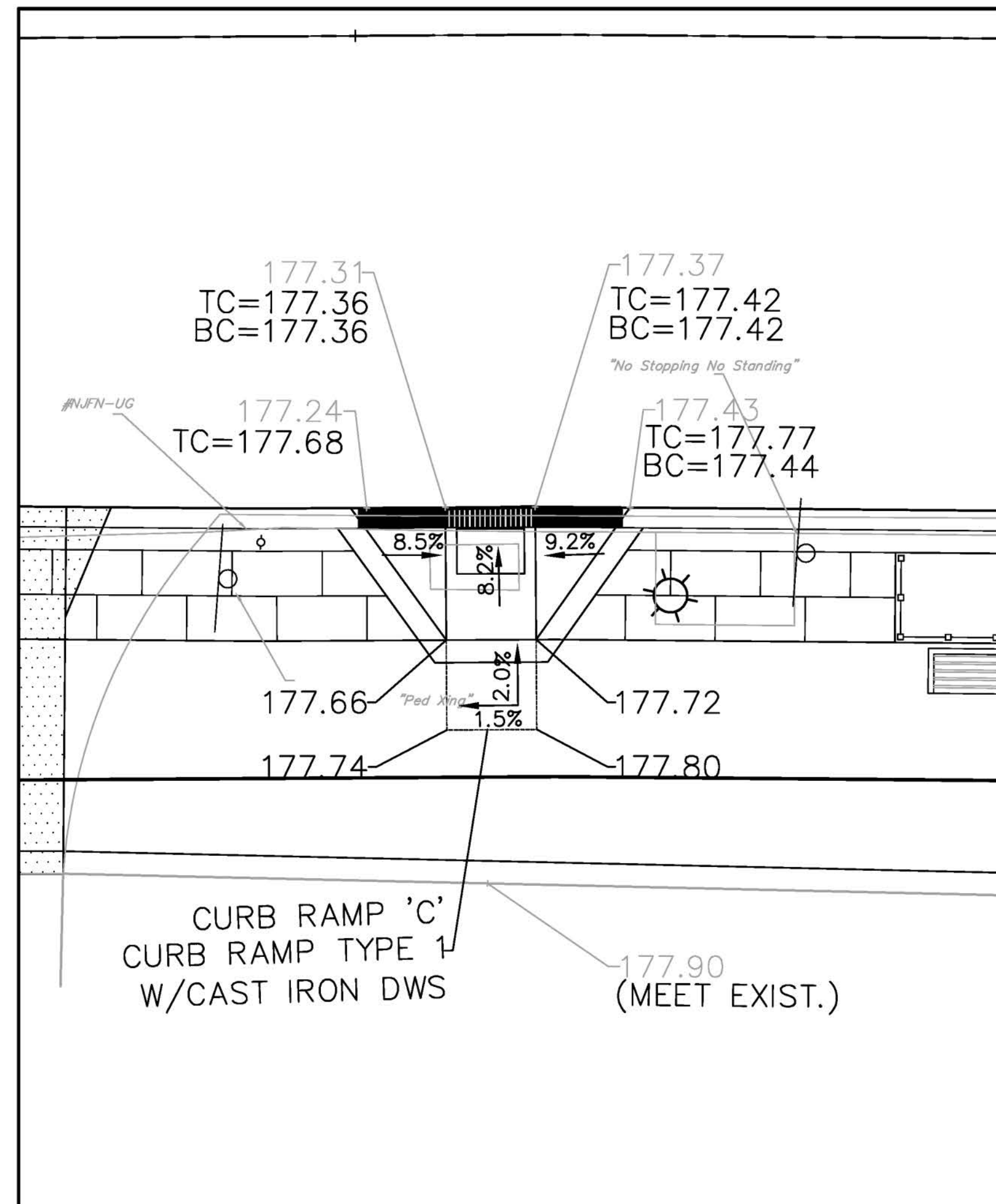
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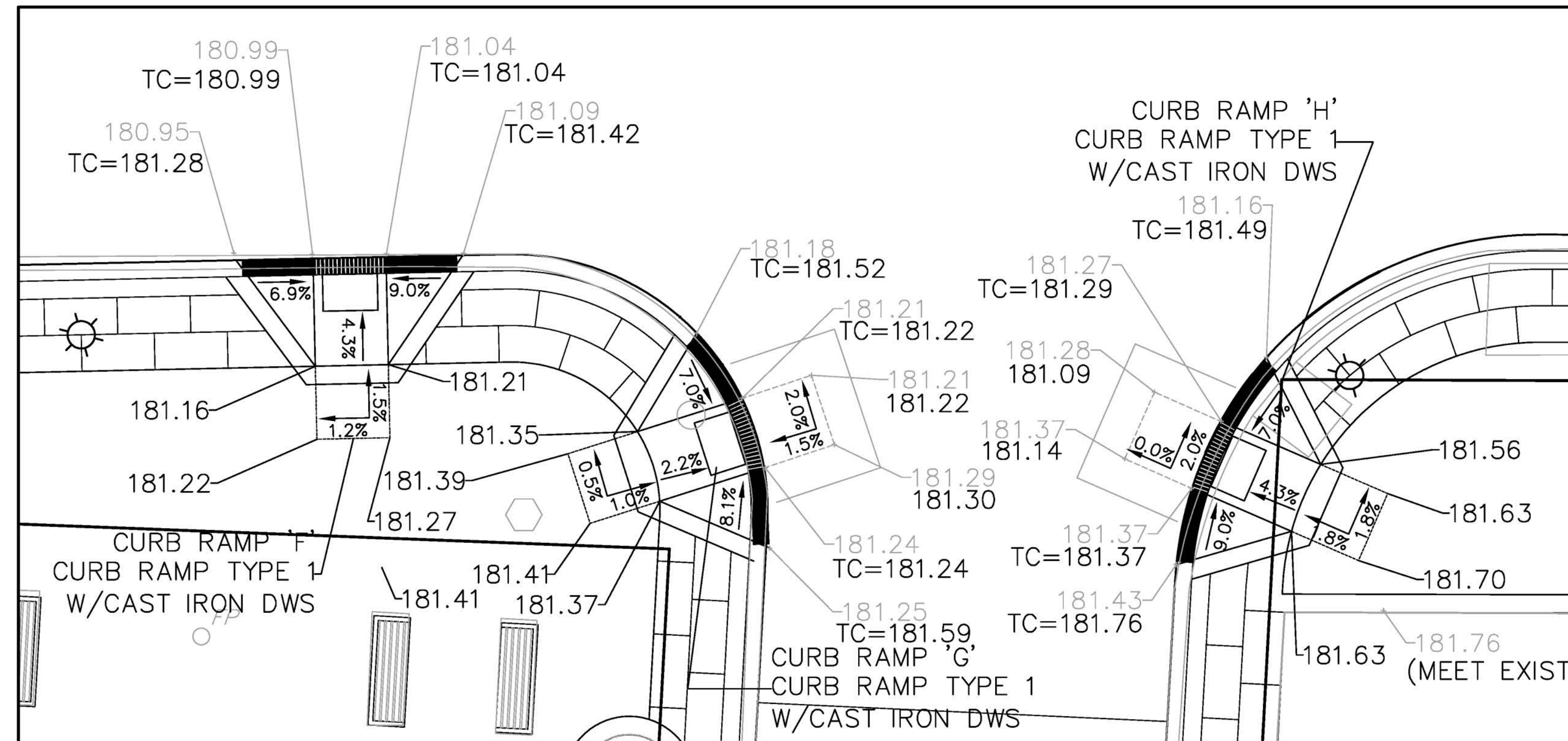
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ACCESSIBLE CURB RAMP D



ACCESSIBLE CURB RAMP C

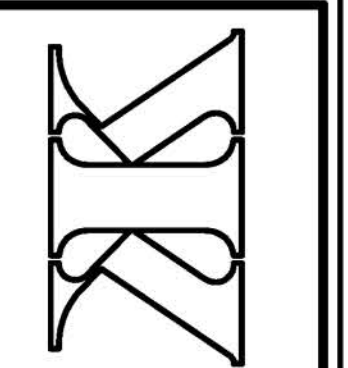


ACCESSIBLE CURB RAMPS F, G & H

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3	NO REVISION THIS SHEET	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/29/18
1	PER MDDOT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17 SCALE: 1" = 5' FLD. BK.: 75-6
 PROJ.: 2016020 CHKD.: PAGE: 28
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Certificate of Authorization #22600027400
 SOI Gibraltar Drive, Suite 204, Morris Plains, NJ 07960 973-377-6600
Keller & Kirkpatrick Inc.
 CONSULTING ENGINEERS
 LAND SURVEYORS
**LANDSCAPE ARCHITECTS
 PLANNERS**
Andrew G. Kangiano
 ANDREW KANGIANO, Professional Engineer, Lic. 43111

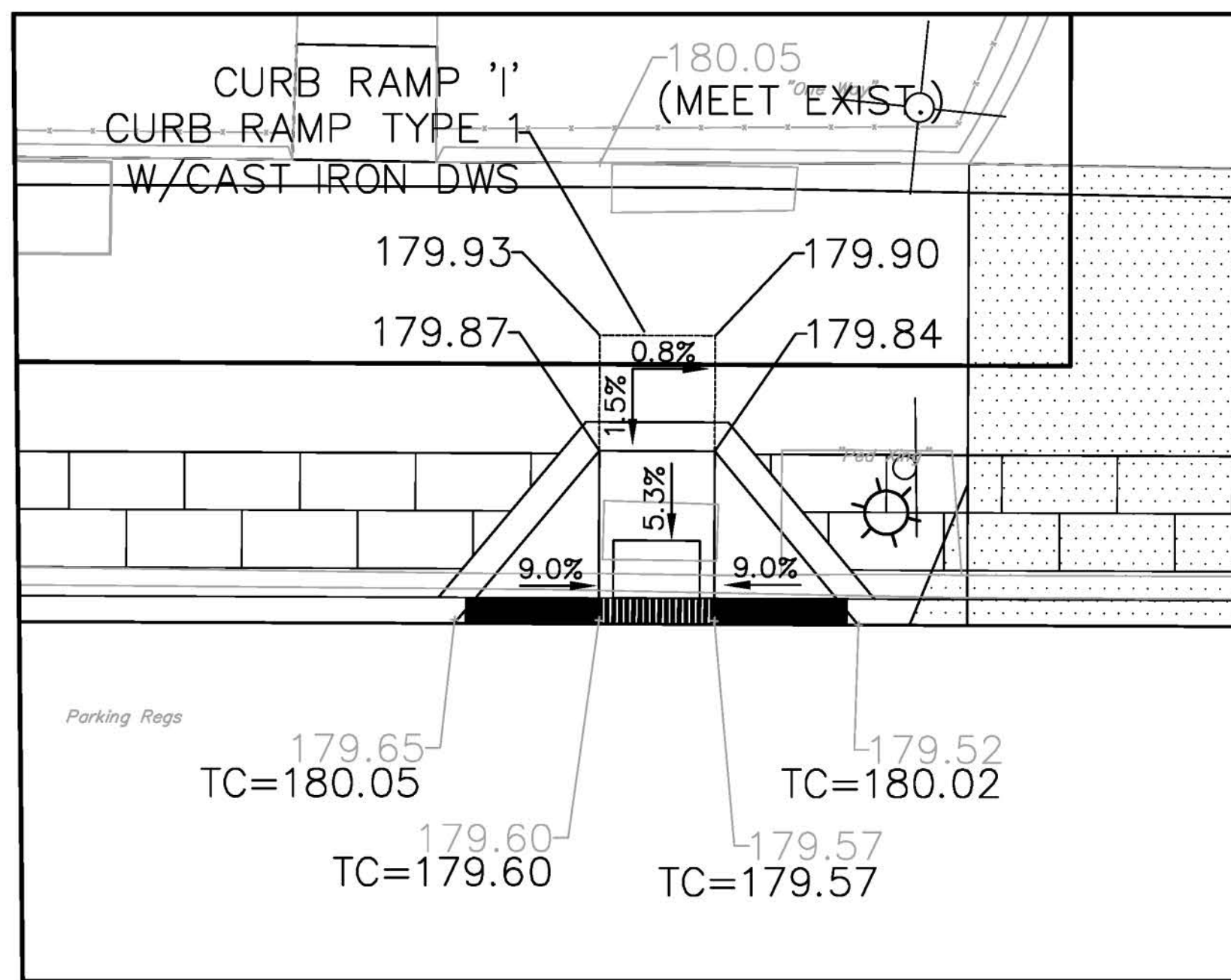


MAIN STREET SIDEWALK & STREETSCAPE
 BLOOMFIELD AVENUE TO MINE STREET
ACCESSIBLE CURB RAMP DETAILS
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

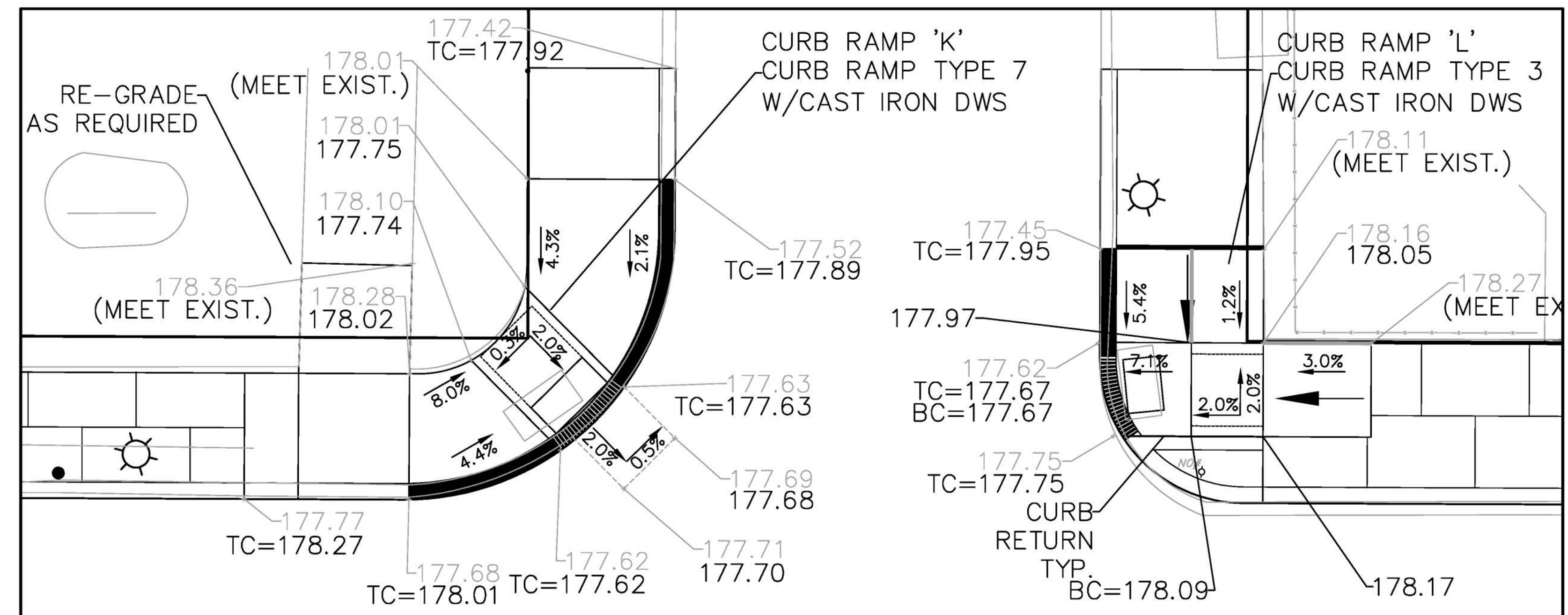
SHEET No.
12
 OF **31**

SEE SHEET 2 OF 31 FOR GENERAL NOTES

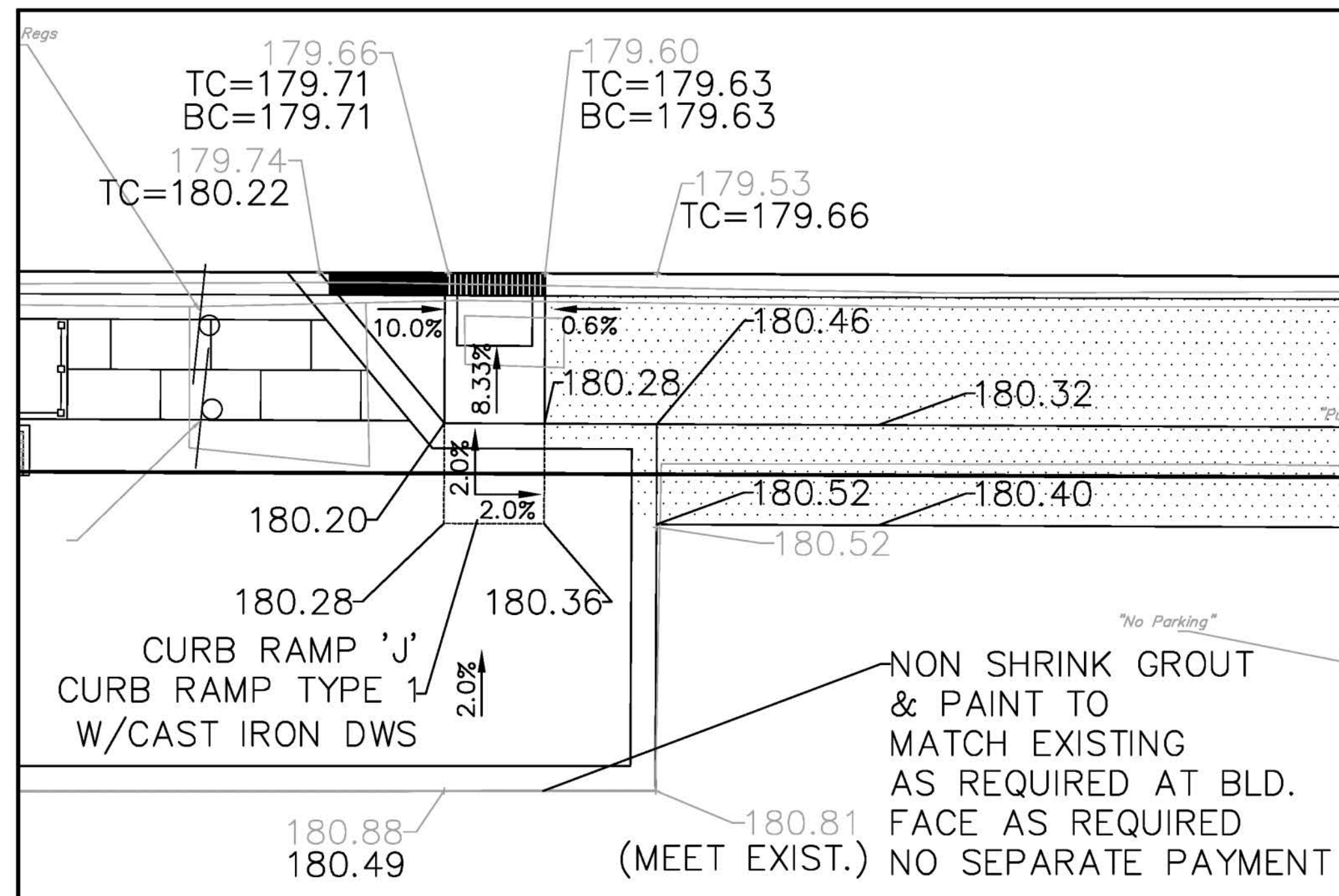
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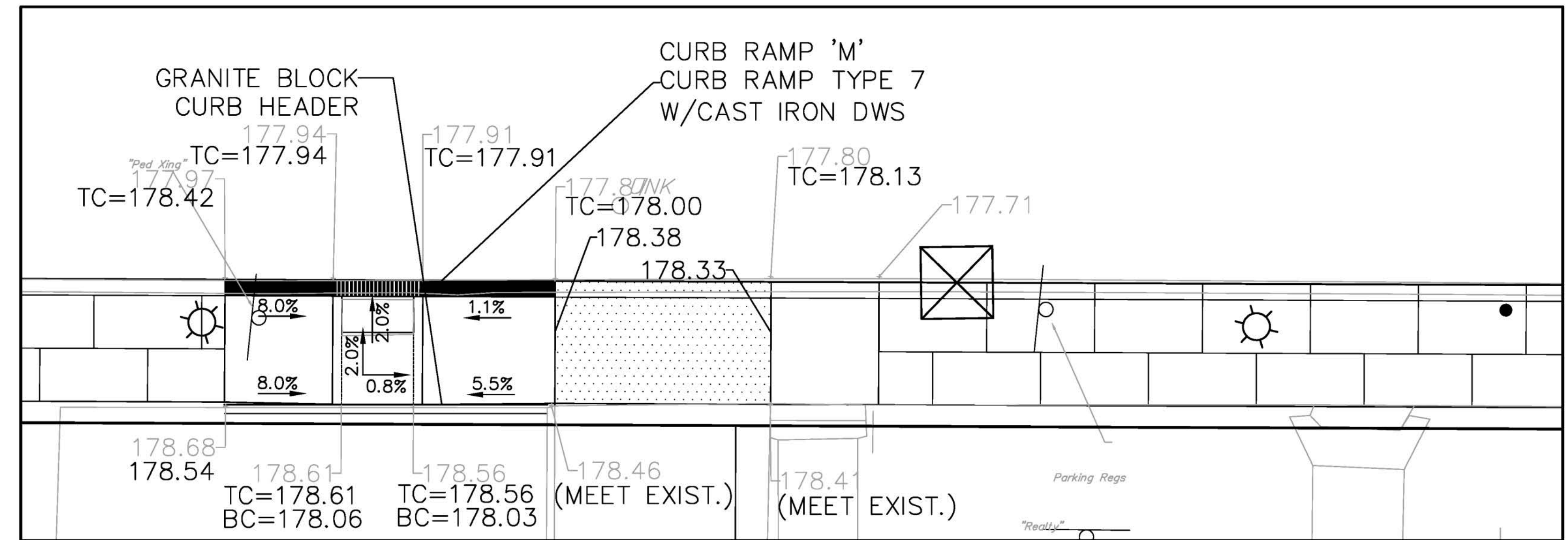
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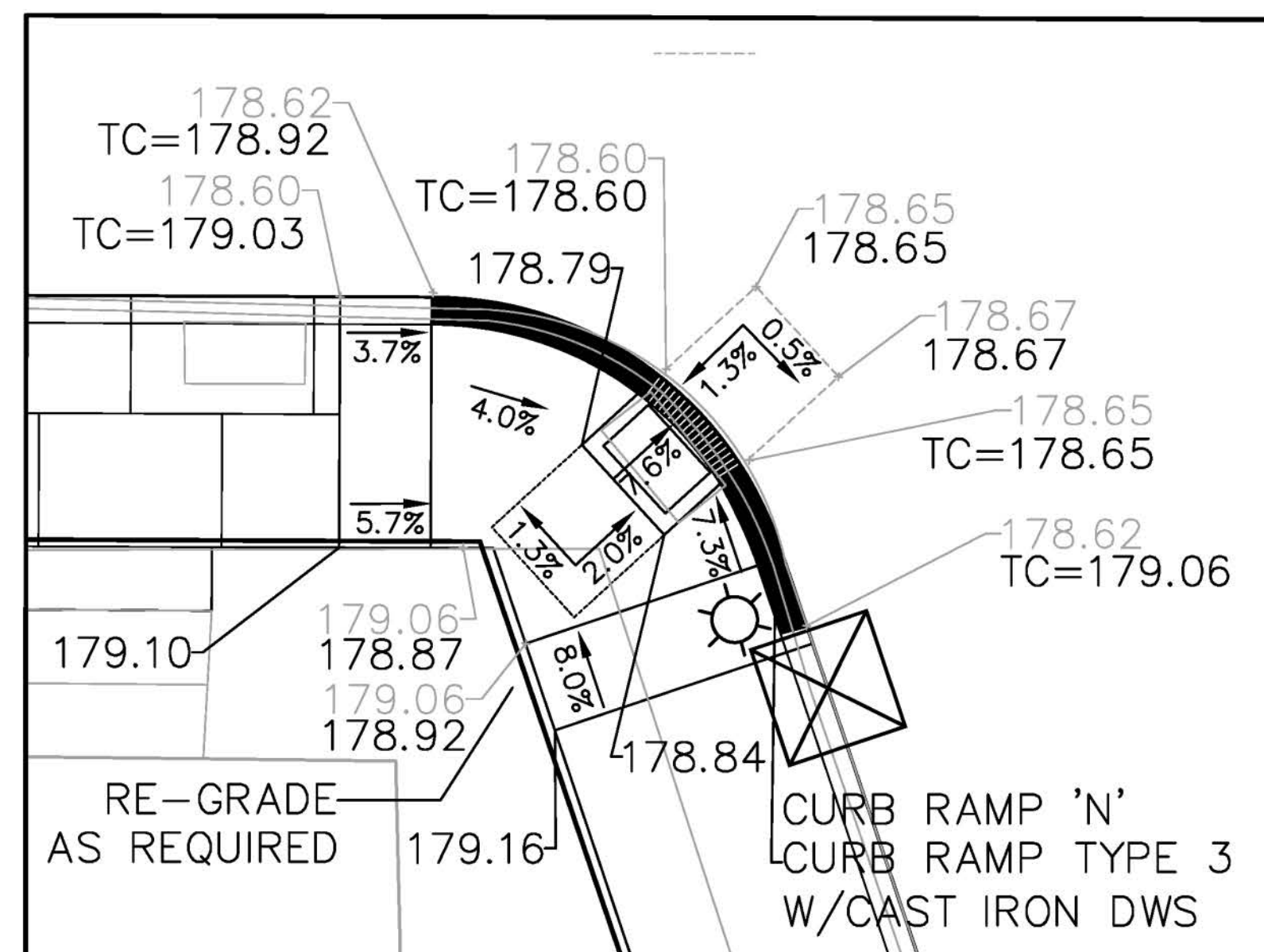
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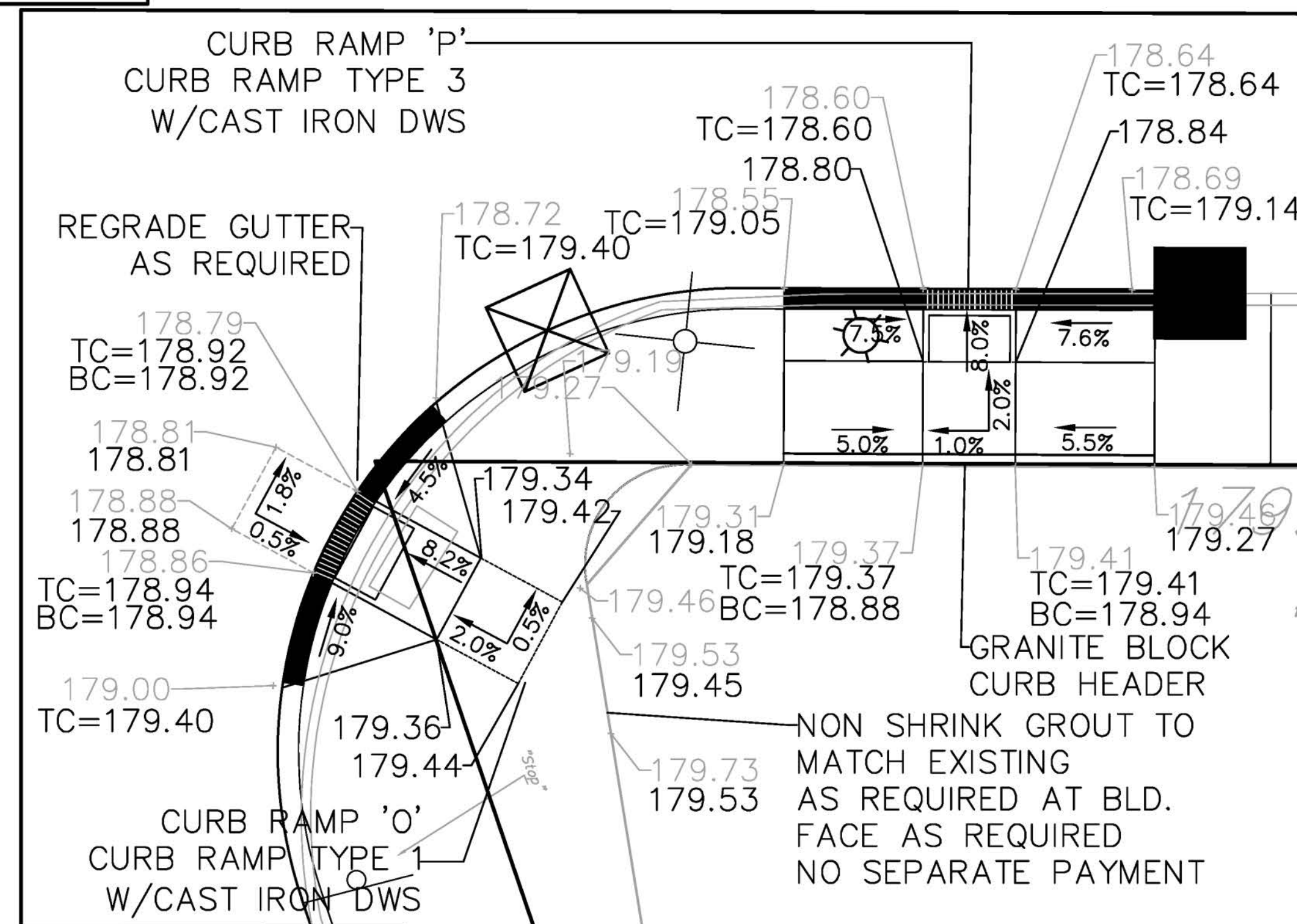
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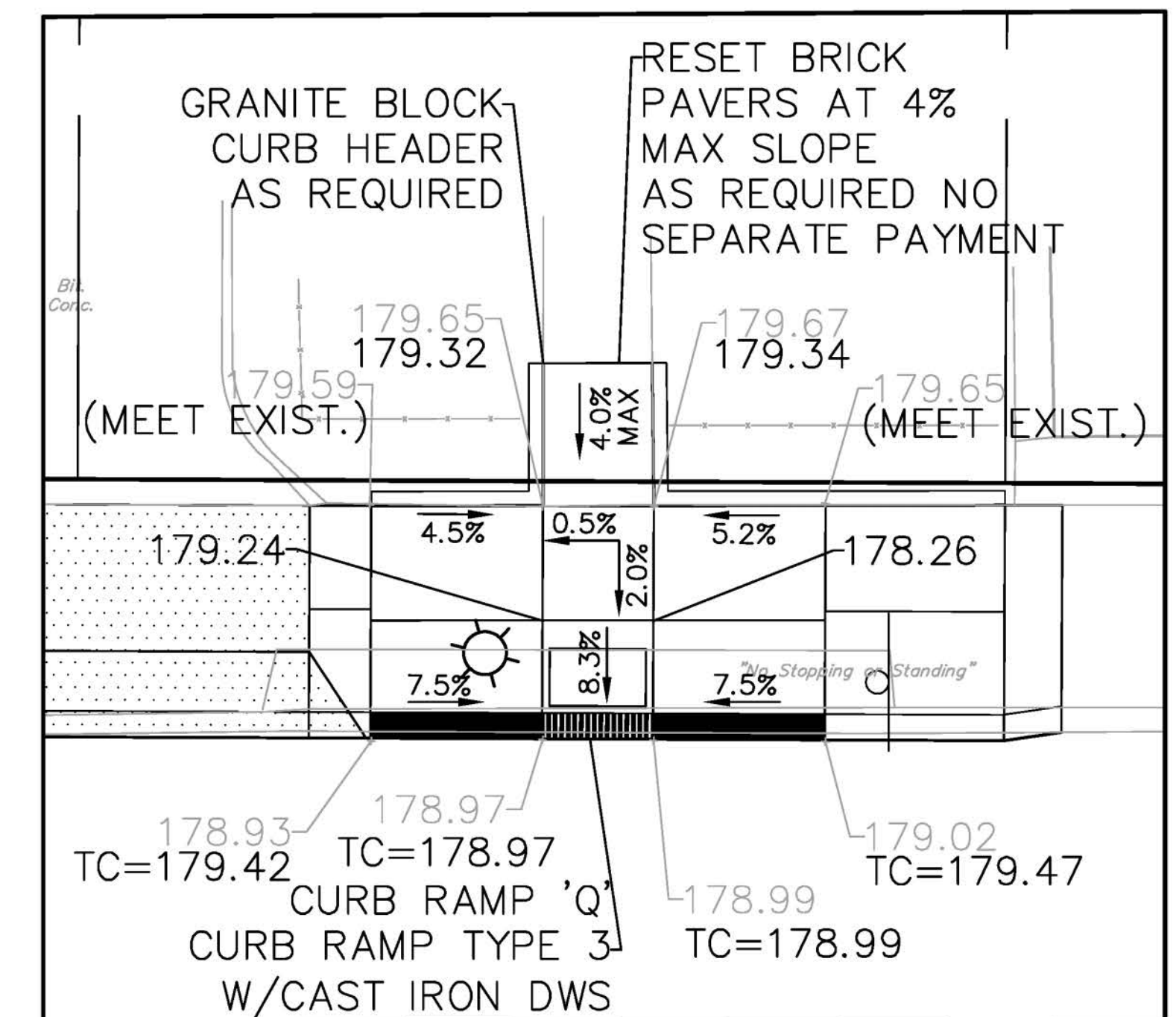
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ACCESSIBLE CURB RAMP N



ACCESSIBLE CURB RAMPS O & P



ACCESSIBLE CURB RAMP Q

SEE SHEET 2 OF 31 FOR GENERAL NOTES

6				
5				
4	NO REVISION THIS SHEET	6/13/18		
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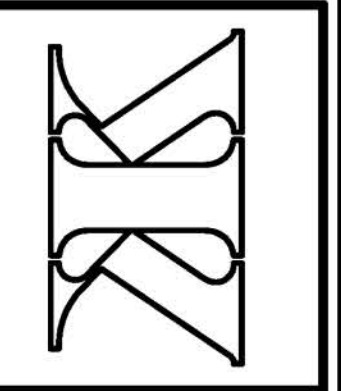
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PROJ.: 2016020	CHKD.:	PAGE: 28

Keller & Kirkpatrick Inc.
CONSULTING ENGINEERS
LAND SURVEYORS

2260027400
SUN Glenburn Drive, Suite 204, Morris Plains, NJ 07960 973-377-6600

LANDSCAPE ARCHITECTS
PLANNERS

ANDREW CANGIANO, Professional Engineer, Lic. 43111



MAIN STREET SIDEWALK & STREETScape
BLOOMFIELD AVENUE TO MINE STREET

ACCESSIBLE CURB RAMP DETAILS

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
FEDERAL AID PROJECT #TAP-1006(335)

SHEET No. **13** OF **31**

GENERAL NOTES (apply to all drawings):

- VERIFY ALL ELECTRICAL REQUIREMENTS AND EXACT LOCATIONS OF EQUIPMENT WITH DRAWINGS AND SPECIFICATIONS.
- SEE ALL SPECIFICATION SECTIONS FOR ADDITIONAL ELECTRICAL WORK REQUIRED, NOT COMPLETELY SHOWN ON DRAWINGS.
- ALL MOUNTING HEIGHTS INDICATED ARE TO CENTERLINES OF DEVICES, EXCEPT FOR LIGHTING FIXTURES AND AS OTHERWISE NOTED OR SPECIFIED. MOUNTING HEIGHTS FOR LIGHTING FIXTURES ARE TO BOTTOM OF UNITS.
- ALL 120 VOLT, 20 AMP OUTLETS LOCATED OUTDOORS SHALL HAVE GFCI PROTECTION.
- PROVIDE ALL GROUNDING REQUIREMENTS AS OUTLINED IN THE NATIONAL ELECTRIC CODE.
- CONTRACTOR HEREBY CAUTIONED THAT ELECTRIC POWER CHARACTERISTICS (VOLTAGE, PHASE, HORSEPOWER, AMPERAGE, ETC) OF EQUIPMENT IS BASED ON AVAILABLE INFORMATION AT THE TIME OF PROJECT DESIGN. CONTRACTOR MUST VERIFY CHARACTERISTICS FOR EACH PIECE OF EQUIPMENT PRIOR TO ORDERING ELECTRICAL DEVICES. INDICATE VERIFICATION ON SUBMITTAL.
- COORDINATION BETWEEN ITEMS REQUIRING ELECTRIC CONNECTIONS AND EXACT FINAL LOCATIONS MUST BE CONSIDERED PRIOR TO FINAL ROUGH IN OF DEVICES.
- UPSIZE WIRE WHERE NECESSARY FOR VOLTAGE DROP PER THE LATEST EDITION OF THE NATIONAL ELECTRIC CODE.
- IF NO PANEL IS DESIGNATED ON A CIRCUIT HOMERUN, OR A CIRCUIT HOMERUN IS NOT SHOWN FOR AN ITEM OF EQUIPMENT REQUIRING POWER (INCLUDING EQUIPMENT NOT SHOWN ON DRAWINGS), ELECTRICAL CONTRACTOR SHALL CIRCUIT TO THE NEAREST AVAILABLE PANEL WITH WIRE AND RACEWAY SIZED PER THE CURRENT NEC. ELECTRICAL CONTRACTOR SHALL ALSO PROVIDE PROPERLY SIZED CIRCUIT BREAKER.
- REFER TO SPECIFICATIONS FOR REQUIREMENTS OF COORDINATION DRAWINGS TO BE COORDINATED WITH OTHER TRADES.

LIGHTING FIXTURE SCHEDULE							
TYPE	MANUFACTURER	CATALOG NUMBER	LAMPS			MOUNTING	NOTES/REMARKS
			WATTAGE	TYPE	VOLTS		
A	STERNBERG	MS805B LED MAINSTREET LUMINAIRE WITH 9400 (14) MARSHALL SERIES POLE CAT#PT-MS805-ALED-BD/5-4ARL35T3-MDL03-HL-240-BKT-CSA-FHC-SHPA-B-8414FPS-MDDA3	66W	LED 3500K	240	POLE	<ul style="list-style-type: none"> PROVIDE WITH HI-LOW CONTROL OPTION. SEE DETAIL. PROVIDE WITH INTEGRAL HOLIDAY LIGHTING RECEPTACLE (GF/MP IN-USE COVER). PROVIDE PHOTOCELL OPTION FOR (1) POLE ON EACH SIDE OF THE ROAD.

NOTES:

- THE LIGHTING FIXTURES LISTED IN THIS SCHEDULE ARE THE "BASIS OF DESIGN". ANY SUBSTITUTIONS TO THOSE LISTED IN THE SCHEDULE MUST BE SUBMITTED BY THE CONTRACTOR TO THE ENGINEER, FOR REVIEW AND EITHER APPROVAL OR REJECTION.
- IN ADDITION TO ALL OF THE REQUIREMENTS OF NOTE #1 ABOVE, EVERY SUBSTITUTION MUST BE PROVIDED WITH A COMPLETE TECHNICAL DESCRIPTIONS.
- THE ENGINEER WILL REVIEW ONLY THOSE SUBSTITUTIONS THAT ARE SUBMITTED IN EXACT ACCORDANCE WITH NOTES #1 AND #2 ABOVE. THE ENGINEER RESERVES THE RIGHT TO REJECT ANY AND ALL SUBSTITUTIONS.

LIGHTING FIXTURE SCHEDULE NOTES:

- PROVIDE LIGHT FIXTURES AT VOLTAGE SPECIFIED IN SCHEDULE SHOWN ABOVE.
- ANY LUMINAIRE OFFERED THAT IS NOT SPECIFIED IN THIS FIXTURE SCHEDULE BY MANUFACTURER AND CATALOG NUMBER SHALL BE ACCOMPANIED BY A FULL DESCRIPTION THAT INCLUDES DRAWINGS OR PHOTOGRAPHS AND INDEPENDENT TEST LAB PERFORMANCE DATA. THE PER-UNIT ADD OR DEDUCT COST SHALL ACCOMPANY THE BID.

ELECTRICAL SYMBOLS

- PHOTOCELL SENSOR
- TAMPER RESISTANT DUPLEX GROUND FAULT CIRCUIT INTERRUPTER 20A RECEPTACLE, WP - WEATHER PROOF DEVICE SHALL BE WEATHER PROOF "IN-USE"
- JUNCTION BOX, WITH BLANK COVERPLATE, CEILING MOUNTED
- UNDERGROUND ELECTRICAL BRANCH CIRCUIT
- BRANCH CIRCUIT HOMERUN WITH PANEL AND CIRCUIT NUMBER. NUMBER OF CROSSLINES INDICATE NUMBER OF BRANCH WIRES INCLUDING GROUND WIRES, EX. 2#12, 1#12 NEUTRAL, 1#12G.
- SITE LIGHTING FIXTURE, POLE MOUNTED. LETTER INDICATES FIXTURE TYPE. SEE LIGHTING FIXTURE SCHEDULE FOR ADDITIONAL DETAILS.
- EXISTING OVERHEAD ELECTRIC UTILITY POLE. COORDINATE EXACT LOCATIONS IN THE FIELD.

LIGHTING CONTROL DESIGN INTENT:

THE CONTRACTOR SHALL FURNISH, INSTALL, AND PROGRAM THE LIGHTING CONTROL TO MEET ALL REQUIREMENTS IN THESE CONSTRUCTION DOCUMENTS AND AS STATED BELOW. UPON COMPLETION OF WORK THE CONTRACTOR SHALL DEMONSTRATE TO THE OWNER AND ENGINEER THAT THE LIGHTING CONTROL MEETS THE DESIGN INTENT SPECIFIED BELOW. DRAWINGS SHOW DESIGN INTENT ONLY. EXACT REQUIREMENTS, DETAILS, CONTROL DIAGRAMS, MATERIALS, ETC. SHALL BE COORDINATED WITH ALL MANUFACTURERS TO PROVIDE A WORKING LIGHTING CONTROL SYSTEM THAT MEETS ALL REQUIREMENTS STATED BELOW. FINAL CONTROL INTENT SHALL BE SUBMITTED TO FOR FINAL REVIEW AND APPROVAL. THE OWNER RESERVES THE RIGHT TO CHANGE CONTROL INTENT TO BETTER MEET THE OWNER'S NEEDS.

- DARK - 10:00PM**
- LIGHT FIXTURES SHALL TURN ON WITH 100% LIGHT OUTPUT.
 - POLE MOUNTED RECEPTACLES SHALL TURN ON.

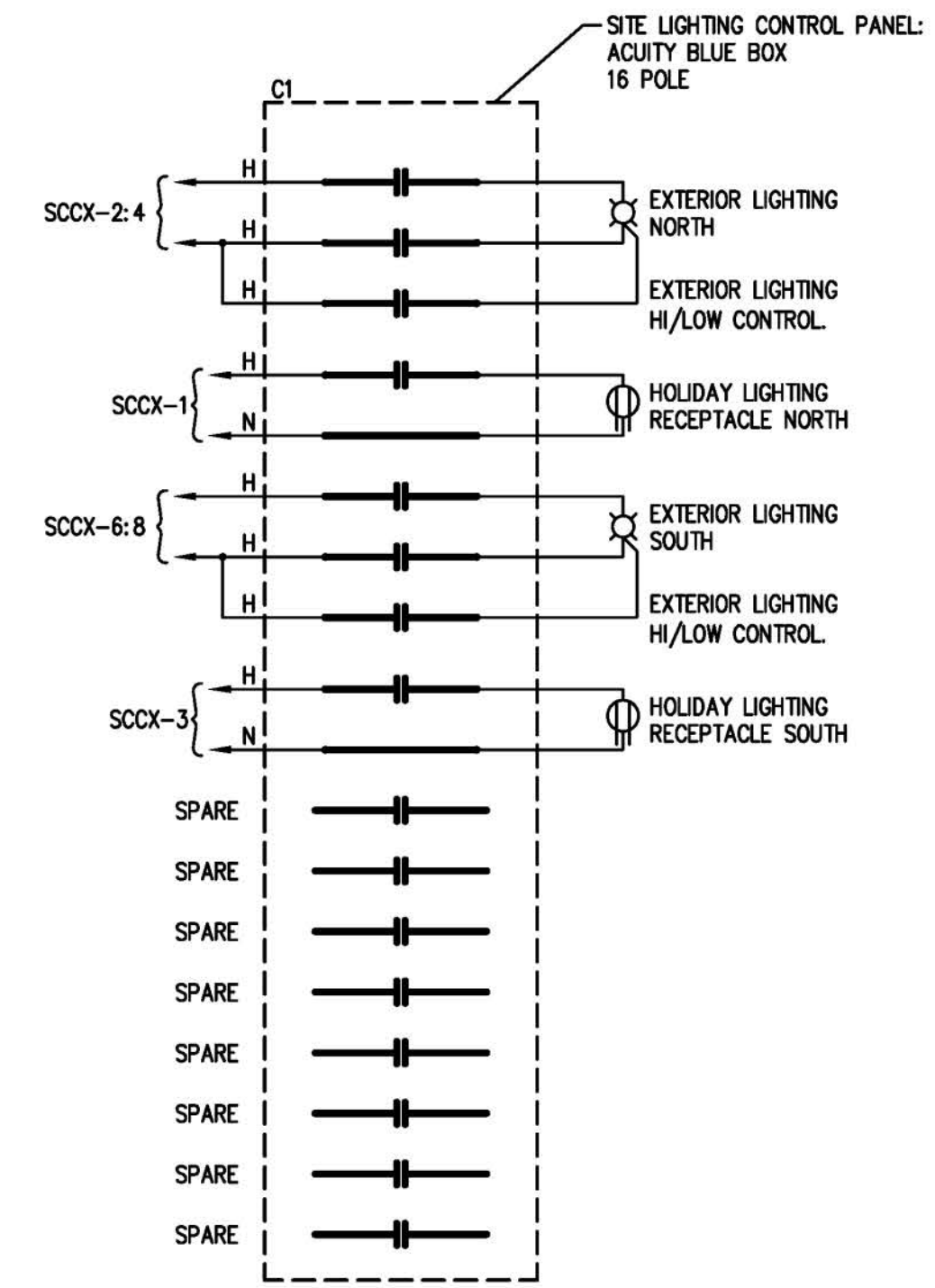
- 11:00PM - 5:00AM**
- LIGHT FIXTURES SHALL DIM DOWN TO 50% LIGHT OUTPUT.
 - POLE MOUNTED RECEPTACLES SHALL STAY ON.

5:01AM - NORMAL DAYLIGHT HOURS

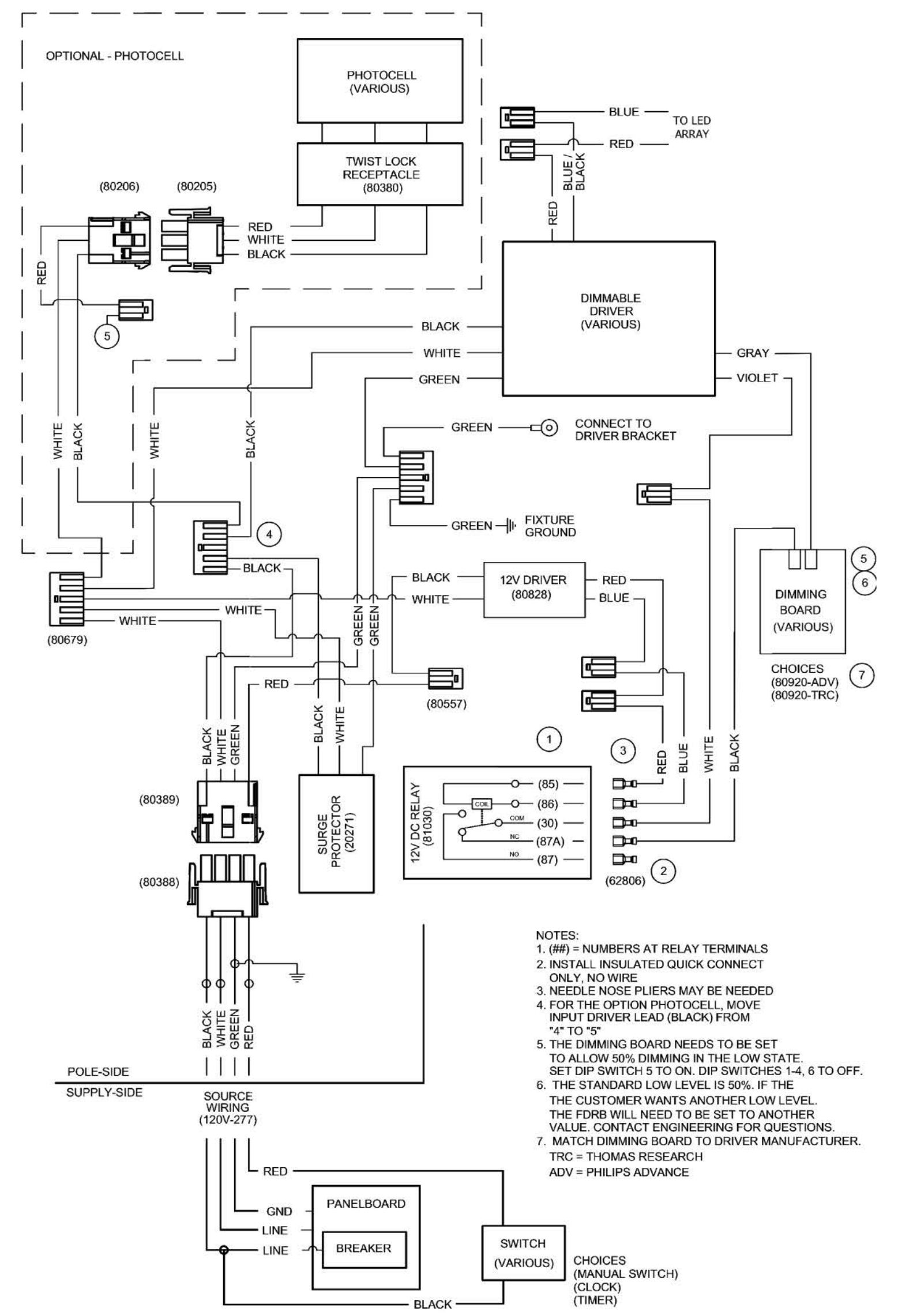
- LIGHT FIXTURES SHALL TURN OFF.
- POLE MOUNTED RECEPTACLE SHALL TURN OFF.
- LIGHT FIXTURES SHALL TURN ON WITH 100% LIGHT OUTPUT IF LIGHT LEVELS DROP (VA PHOTOCELL).

NOTES:

- LIGHTING SCHEDULE SHALL AUTOMATICALLY ADJUST WITH TIME OF YEAR & DAYLIGHT SAVINGS TIME.
- PROVIDE LOCAL OVERRIDE SWITCH IN CONTROL CABINET FOR LIGHT CIRCUITS.
- PROVIDE LOCAL OVERRIDE SWITCH IN CONTROL CABINET FOR RECEPTACLE CIRCUITS.
- PROVIDE PHOTOCELL MOUNTED ON FIRST POLE IN LIGHTING CIRCUIT FOR DAYTIME ON/OFF.



SITE LIGHTING CONTROL DIAGRAM
SCALE: NONE



STERNBERG HI - LOW WIRING DIAGRAM
SCALE: NONE

ELECTRICAL ABBREVIATIONS

1/C..... SINGLE CONDUCTOR	GSR..... GROUND SENSING RELAY
A, AMP..... AMPERE	HD..... HAND DRYER
AC..... ALTERNATING CURRENT	HT..... HEIGHT
A/C..... AIR CONDITIONER	HD..... HIGH INTENSITY DISCHARGE
ADD..... ADDITIONAL	HP..... HORSEPOWER
AF..... AMP FRAME	HPS..... HIGH PRESSURE SODIUM
A.F.F..... ABOVE FINISHED FLOOR	HZ..... HERTZ
AFG..... ABOVE FINISHED GRADE	IMC..... INTERMEDIATE METAL CONDUIT
ARF..... ABOVE RAISED FLOOR	IN..... INCH
AHU..... AIR HANDLING UNIT	INCAND..... INCANDESCENT
AIC..... INTERRUPTING CAPACITY (AMPERES)	INSUL..... INSULATION
AL..... ALUMINUM	INV..... INVERT
APPROX..... APPROXIMATELY	JB..... JUNCTION BOX
ARCH..... ARCHITECTURAL	KCMIL..... THOUSAND CIRCULAR MILLS
ASY..... ASYMMETRICAL	KW..... KILOWATT
ATC..... AUTOMATIC TEMPERATURE CONTROL	KWH..... KILOWATT HOUR
ATS..... AUTOMATIC TRANSFER SWITCH	KVA..... KILOVOLT AMPERE
AUX..... AUXILIARY	KV..... KILOVOLT
AT..... AMP TRIP	LG..... LONG
AWG..... AMERICAN WIRE GAUGE	LT..... LIGHT
BFC..... BELOW FINISHED CEILING	LTG..... LIGHTING
BFG..... BELOW FINISHED GRADE	M/C..... MULTI CONDUCTOR
BLDG..... BUILDING	MCC..... MOTOR CONTROL CENTER
BKR..... BREAKER	MCB..... MAIN CIRCUIT BREAKER
BMS..... BUILDING MANAGEMENT SYSTEM	MDP..... MAIN DISTRIBUTION PANEL
BR..... BRANCH	MH..... MANHOLE
C..... CONDUIT	MIN..... MINIMUM
COND..... CONDUIT	MLO..... MAIN LUGS ONLY
CAS..... CARD ACCESS SYSTEM	MMFO..... MULTI-MODE FIBER OPTIC
CB..... CIRCUIT BREAKER	MTD..... MOUNTED
CKD..... CIRCUITED	N..... NEUTRAL
CKT..... CIRCUIT	NC..... NORMALLY CLOSED
CLG..... CEILING	NEC..... NATIONAL ELECTRIC CODE
COAX..... COAXIAL	NEMA..... NATIONAL ELECTRICAL MANUFACTURERS ASSOCIATION
CONC..... CONCRETE	CONTR..... CONTRACTOR
CONTR..... CONTRACTOR	CT..... CURRENT TRANSFORMER
CU..... COPPER	DB..... DIRECT BURIAL
DB..... DIRECT BURIAL	DC..... DIRECT CURRENT
DC..... DIRECT CURRENT	DISC..... DISCONNECT
DISC..... DISCONNECT	DIV..... DIVISION
DIV..... DIVISION	DWG..... DRAWING
DWG..... DRAWING	EA..... EACH
EA..... EACH	E.C..... ELECTRICAL CONTRACTOR
E.C..... ELECTRICAL CONTRACTOR	EF..... EXHAUST FAN
EF..... EXHAUST FAN	EL..... ELEVATION
EL..... ELEVATION	ELEC..... ELECTRIC
ELEC..... ELECTRIC	ELEV..... ELEVATOR
ELEV..... ELEVATOR	EMERG..... EMERGENCY
EMERG..... EMERGENCY	EMT..... ELECTRICAL METALLIC TUBING
EMT..... ELECTRICAL METALLIC TUBING	ENCL..... ENCLOSURE
ENCL..... ENCLOSURE	EQUIP..... EQUIPMENT
EQUIP..... EQUIPMENT	EWC..... ELECTRIC WATER COOLER
EWC..... ELECTRIC WATER COOLER	EXIST..... EXISTING
EXIST..... EXISTING	EXT..... EXTERNAL / EXTERIOR
EXT..... EXTERNAL / EXTERIOR	F..... FLUSH
F..... FLUSH	FAAN..... FIRE ALARM ANNUNCIATOR PANEL
FAAN..... FIRE ALARM ANNUNCIATOR PANEL	FACP..... FIRE ALARM CONTROL PANEL
FACP..... FIRE ALARM CONTROL PANEL	FA..... FIRE ALARM
FA..... FIRE ALARM	FCU..... FAN COIL UNIT
FCU..... FAN COIL UNIT	FDR..... FEEDER
FDR..... FEEDER	FIN..... FINISHED
FIN..... FINISHED	FIXT..... FIXTURE
FIXT..... FIXTURE	FL..... FLOOR
FL..... FLOOR	FLA..... FULL LOAD AMPS
FLA..... FULL LOAD AMPS	FLEX..... FLEXIBLE
FLEX..... FLEXIBLE	FLUOR..... FLUORESCENT
FLUOR..... FLUORESCENT	FO..... FIBER OPTIC
FO..... FIBER OPTIC	FS..... FLOW SWITCH
FS..... FLOW SWITCH	FT..... FOOT
FT..... FOOT	G.C..... GENERAL CONTRACTOR
G.C..... GENERAL CONTRACTOR	GFCI..... GROUND FAULT CIRCUIT INTERRUPTER
GFCI..... GROUND FAULT CIRCUIT INTERRUPTER	GND, G..... GROUND
GND, G..... GROUND	

GPI
GREENMAN - PEDERSEN, INC.
50 glenmaura national blvd, suite 102
scranton pa, 18505
tel 570.342.3700 | fax 570.342.3080

GPI Certificate of Authorization: 24GA27959500

John Murphy
John D. Murphy, PE
N.J. Professional Engineer No. 24GE04222000
04/24/2017

6/13/18
5/16/18
3/28/18
10/17/17

REVISIONS

DATE: 04/24/17
PROJECT: 2016020
SCALE: NONE
CHKD.: J. M.

F.L.D. BK.:
PAGE:

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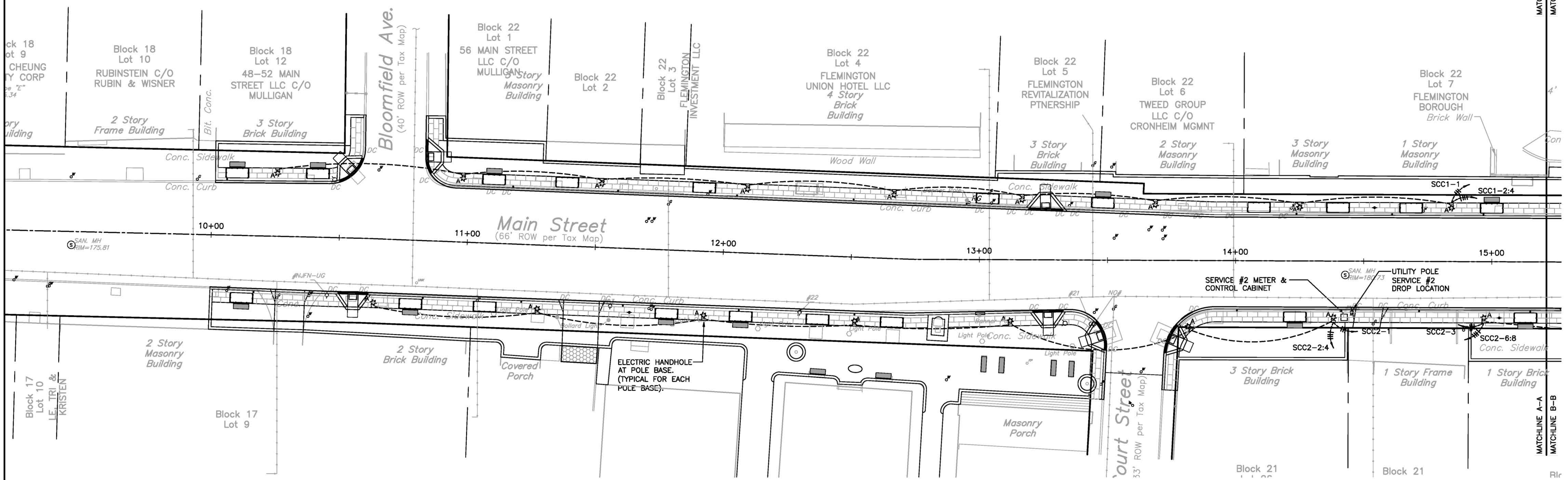
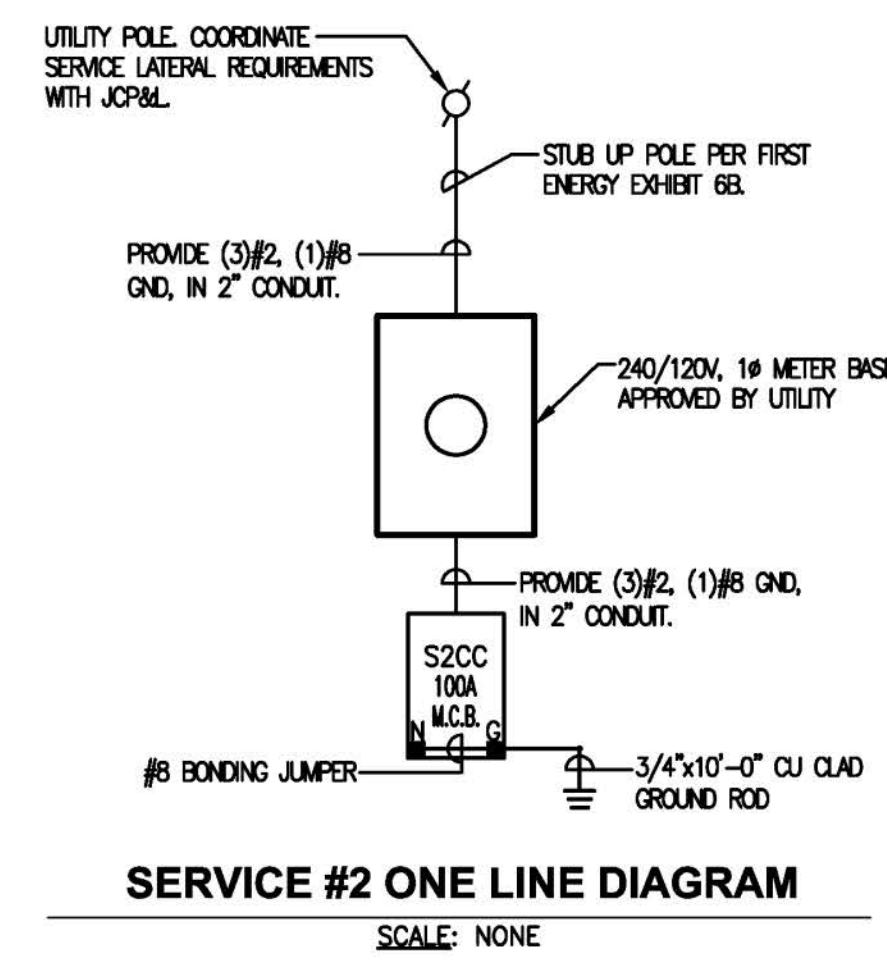
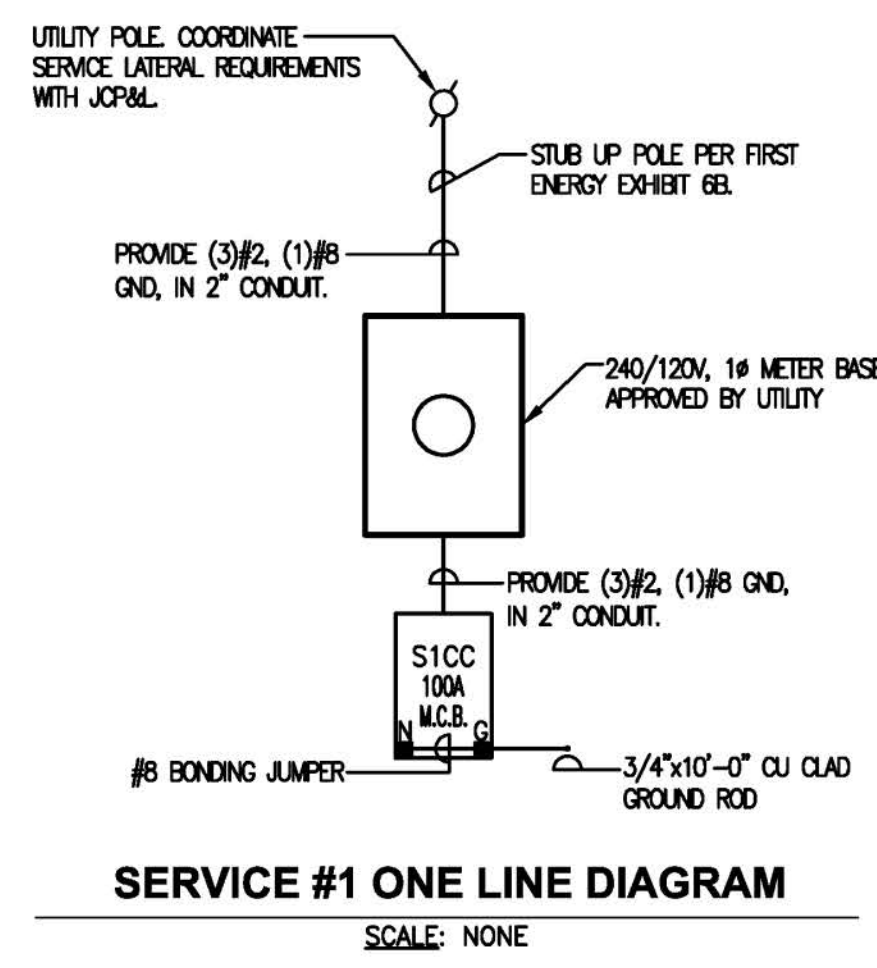
ANDREW CANGIANO, Professional Engineer, Lic. 43111

MAIN STREET SIDEWALK & STREETScape
BLOOMFIELD AVENUE TO MINE STREET
ELECTRICAL SYMBOLS,
LEGEND & DETAILS

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
FEDERAL AID PROJECT #TAP-1006(335)

SHEET No. 14 OF 31

TABLE OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY SHEET 15
34	HANDHOLE/SPLICE BOX	UNITS	17
35	1.5" RIGID NONMETALIC, SCHEDULE 80 PVC	LF	719
36	2.0" RIGID NONMETALIC, SCHEDULE 80 PVC	LF	20
37	FOUNDATION TYPE CUF	UNIT	17
38	FOUNDATION TYPE MC	UNIT	1
39	GROUND WIRE, NO. 8 AWG	LF	1668
40	SERVICE WIRE, NO. 2 AWG	LF	60
41	LIGHTING WIRE, NO. 8 AWG	LF	4170
42	METER CABINET, TYPE T L (POWDER COATED)	UNITS	1
43	LIGHT, STANDARD ALUMINUM	UNITS	17
44	LUMINAIRE	UNITS	17



ELECTRICAL SERVICE NOTES:

CONSTRUCTION PLAN - PROVIDE THE CIVIL ENGINEER WITH A CONSTRUCTION PLAN SHOWING THE PROPOSED LOCATION OF THE UNDERGROUND DUCT SYSTEM. ANY DEVIATIONS FROM THE PROPOSED DUCT ROUTE ON THE DRAWINGS, DUE TO ON-SITE OBSTRUCTIONS, MUST BE APPROVED BY THE CIVIL ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR ANY NECESSARY CROSSING PERMITS.

DEPTH OF BURIAL - ROUGH GRADING TO WITHIN 6 INCHES OF FINAL GRADE SHOULD BE ESTABLISHED BEFORE TRENCH IS DUG. TRENCH MUST BE FREE OF HIGH SPOTS, ROCK PROJECTIONS, AND DEBRIS. MINIMUM DEPTH OF COVER SHALL BE AS FOLLOWS:
 PRIMARY DUCTS -----
 DUCTS UNDER ROADWAY -----

CLEARANCE FROM EXISTING UNDERGROUND FACILITIES - THE CLEARANCE BETWEEN DUCT SYSTEMS AND EXISTING UNDERGROUND FACILITIES (I.V. CABLE, TELEPHONE CABLE, GAS AND WATER ETC.) PARALLELING IT SHALL IN NO CASE BE LESS THAN 24 INCHES UNLESS OTHERWISE SHOWN. DUCT SYSTEMS WHICH PARALLEL A STEAM LINE SHALL HAVE A MINIMUM 6 FEET SEPARATION (CAN BE REDUCED IF DUCTS ARE PROPERLY INSULATED).
 A DUCT SYSTEM WHICH CROSSES OVER OR UNDER EXISTING UNDERGROUND FACILITIES MUST HAVE THE FOLLOWING MINIMUM CLEARANCE:
 TELEPHONE -----
 GAS AND WATER MAINS -----
 SEWERS -----
 STEAM LINES -----

IF THESE CLEARANCES CANNOT BE OBTAINED, INFORM THE CIVIL ENGINEER PRIOR TO CONSTRUCTION.

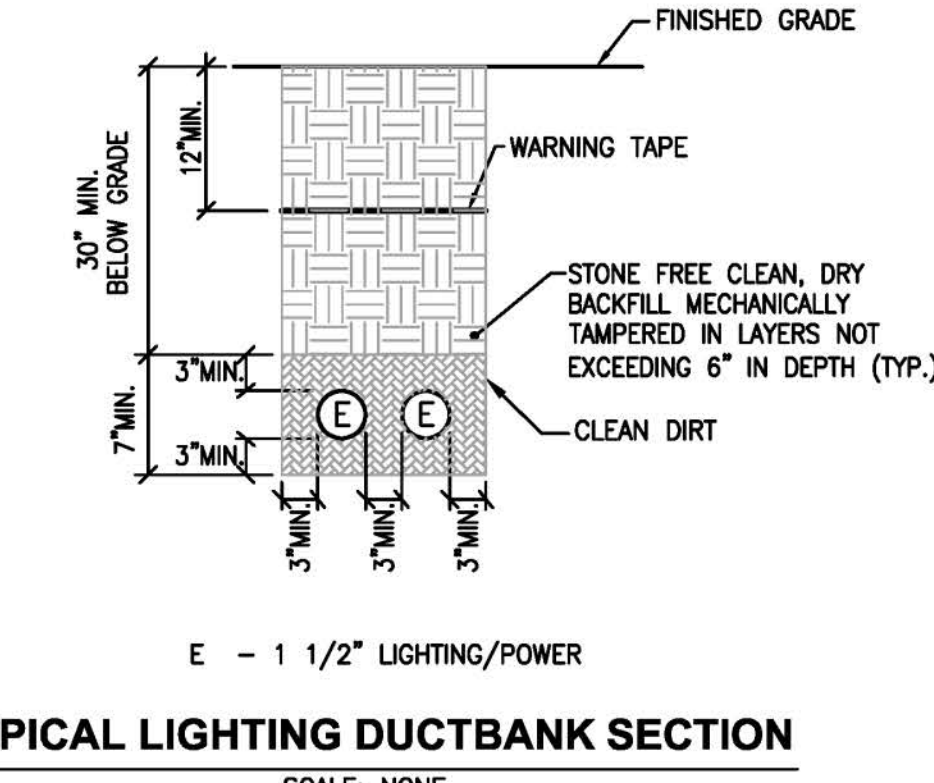
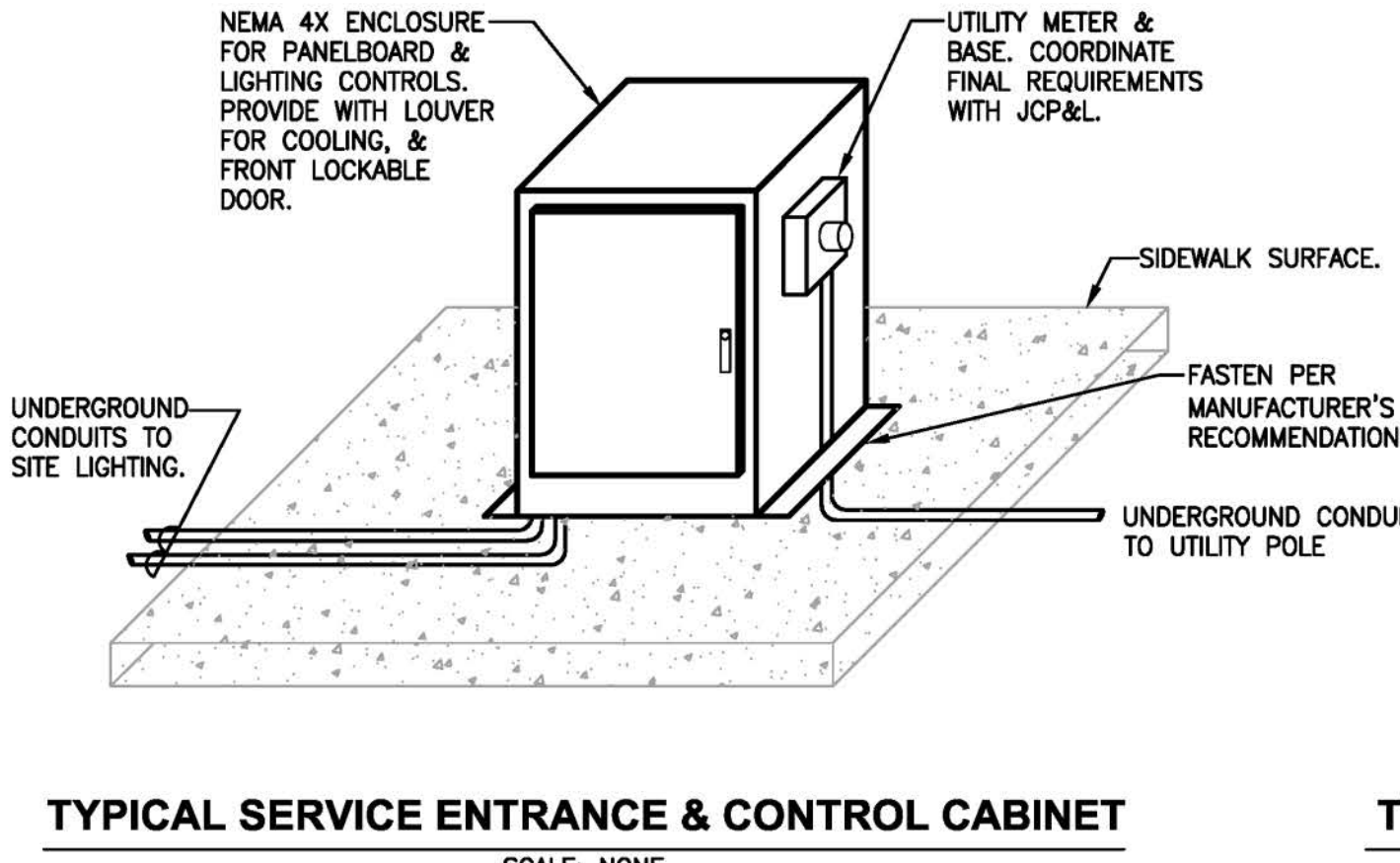
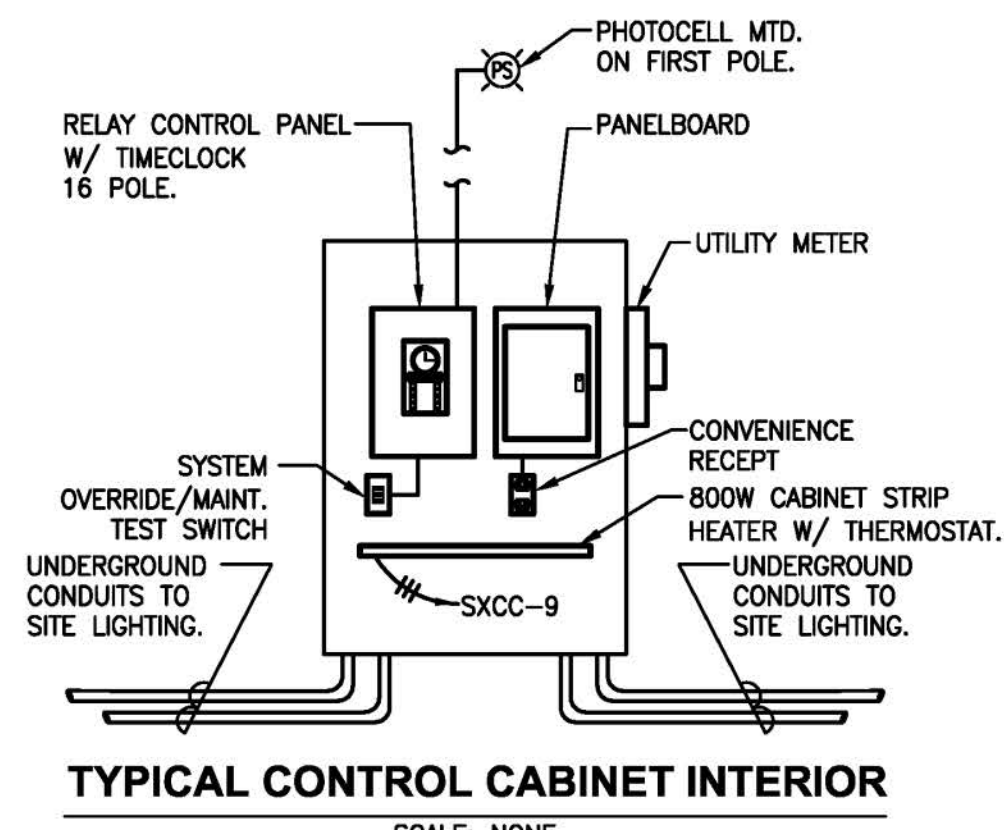
CONDUIT BENDS - BENDS IN 4 INCH CONDUIT SYSTEMS SHALL BE MINIMUM 48 INCH RADIUS. THE CONTRACTOR MAY USE PREFORMED BENDS OF CONDUITS AT THE JOB SITE AS REQUIRED. ALL 90° BENDS MUST BE RIGID OR INTERMEDIATE GRADE GALVANIZED STEEL.

COUPLINGS - ALL PVC CONDUIT COUPLINGS MUST BE SOLIDLY CEMENTED. THREADED JOINTS IN STEEL CONDUIT MUST BE SEALED USING PLUMBER'S TEFLON TAPE.

BELL ENDS - ALL PVC DUCTS ENTERING MANHOLE, VAULTS, HANDHOLES OR TRANSFORMER FOUNDATIONS SHALL HAVE BELL-ENDS. STEEL CONDUITS REQUIRE GROUNDING BUSHINGS.

CLEAN DUCTS - ALL DUCTS SHALL BE CLEANED BY PULLING A MANDREL OR STIFF BRISTLED BRUSH, FOLLOWED BY A BALL OF CLEAN RAGS. THIS PROCEDURE WILL REMOVE EARTH, SAND, STONES OR WATER LEFT IN DUCTS. SEAL ENDS AFTER CLEANING.

PULL WIRE (FISH) - PROVIDE EMPTY CONDUITS WITH A PLASTIC ROPE WITH A BREAKING STRENGTH OF AT LEAST 200 LBS. ALLOW A MINIMUM OF 24 INCHES OF SPARE AT EACH END OF THE CONDUIT RUN.



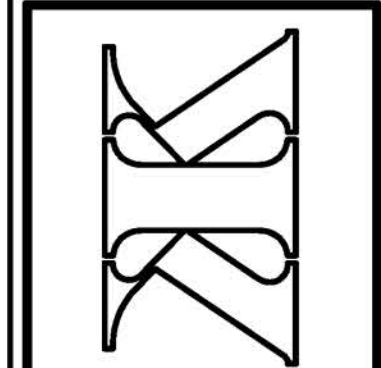
NOTE:
STRIP HEATER SHALL BE CAPABLE OF MAINTAINING A MINIMUM TEMPERATURE OF 40°F WITHIN THE CONTROL CABINET.

NO.	REVISIONS	DATE	BY
6			
5			
4	NO REVISION THIS SHEET	6/13/18	
3	PER SHPO: REMOVE TREES @ UNION HOTEL	5/16/18	
2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18	
1	PER MDDT COMMENT RE: ROW CERTIFICATION	10/17/17	

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Keller & Kirkpatrick Inc.
 501 Gibraltar Drive, Suite 204, Morris Plains, NJ 07960
 973-377-6600
 LANDSCAPE ARCHITECTS
 PLANNERS
 CONSULTING ENGINEERS
 LAND SURVEYORS

Andrew Cangiano
 ANDREW CANGIANO, Professional Engineer, Lic. 43111



MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
PARTIAL ELECTRICAL SITE PLAN

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(C35)

GPI
 GREENMAN - PEDERSEN, INC.
 50 glenmaura national blvd, suite 102
 scranton pa, 18505
 tel 570.342.3700 | fax 570.342.3080

GPI Certificate of Authorization: 24GA27959500

John Murphy
 John D. Murphy, PE
 N.J. Professional Engineer No. 24GE04222000
 04/24/2017

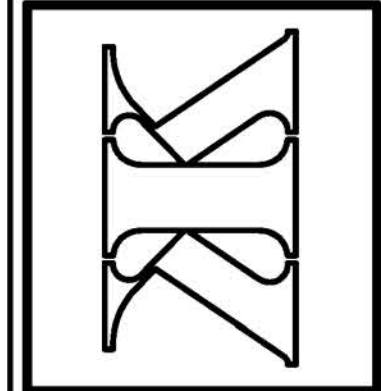
SHEET No.
15
 OF
31

TABLE OF QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
34	HANDHOLE/SPLICE BOX	UNITS	16
35	1.5" RIGID NONMETALIC, SCHEDULE 80 PVC	LF	970
36	2.0" RIGID NONMETALIC, SCHEDULE 80 PVC	LF	20
37	FOUNDATION TYPE OLF	UNIT	16
38	FOUNDATION TYPE MC	UNIT	1
39	GROUND WIRE, NO. 8 AWG	LF	1780
40	SERVICE WIRE, NO. 2 AWG	LF	60
41	LIGHTING WIRE, NO. 8 AWG	LF	4450
42	METER CABINET, TYPE T L (POWDER COATED)	UNITS	1
43	LIGHT, STANDARD ALUMINUM	UNITS	16
44	LUMINAIRE	UNITS	16

NO.	DATE	REVISIONS
6		
5		
4	6/13/18	NO REVISION THIS SHEET
3	5/16/18	NO REVISION THIS SHEET
2	3/28/18	PER SHPO COMMENT RE: CONCRETE CURB
1	10/17/17	PER MDDT COMMENT RE: ROW CERTIFICATION

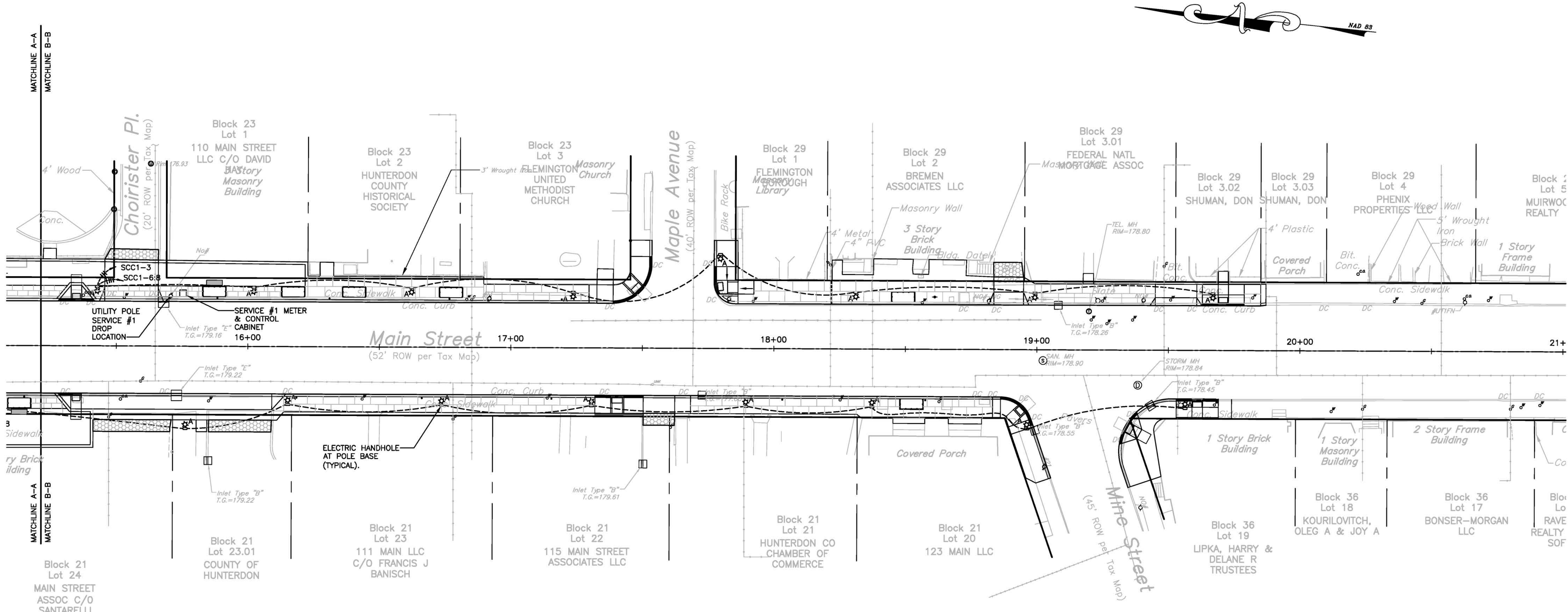
DATE: 04/24/17
 SCALE: 1" = 20'
 FLD. BK.: 75-6
 PROJ.: 2016020
 CHKD.: J. M.
 PAGE: 28

Keller & Kirkpatrick Inc.
 CONSULTING ENGINEERS
 LAND SURVEYORS
 LANDSCAPE ARCHITECTS
 PLANNERS



MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
 PARTIAL ELECTRICAL SITE PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
 16
 31
 OF



PARTIAL SITE PLAN
 SCALE: 1" = 20'-0"

DESIGNATION: S1CC		VOLTAGE: 240/120V, 1ø, 3W		LOCATION: CONTROL CABINET											
MAINS: 100A M.C.B.		FED BY: UTILITY													
TYPE: GENERAL		MINIMUM O.C. DEVICE: 10,000 AC RMS SYM													
O.C. DEVICE: M.C.B. SERVICE ENTRANCE RATED		INTERRUPTING RATING:													
MOUNTING: SURFACE															
Description	COND.	GND.	WIRE NO. SIZE	AMPS	POLES	CKT.	KVA # A	KVA # C	CKT.	POLES	AMPS	WIRE SIZE NO.	GND.	COND.	Description
* RECEPTS: NORTH	1 1/2"	8	2 8	20	1	1	0.72	0.30	2	2	20	1 1/2"			STREET LIGHTS: NORTH
* RECEPTS: SOUTH	1 1/2"	8	2 8	20	1	3		0.72	0.30	4					
LIGHTING CONTROL PANEL	3/4"	12	2 12	20	1	5	0.50	0.26	6	2	20	8 3	8	1 1/2"	STREET LIGHTS: SOUTH
RECEPT: CABINET MID	3/4"	12	2 12	20	1	7		0.18	0.26	8					
CABINET STRIP HEATER	3/4"	12	2 12	20	1	9	0.80		10	2	20				SPARE
SPARE									12						SPARE
SPARE									14	2	20				SPARE
SPARE									16						SPARE
SPARE									18	1	20				SPARE
TOTAL KVA/#							2.58	1.78							
TOTAL KVA							4.36								
TOTAL AMPS							18.17								

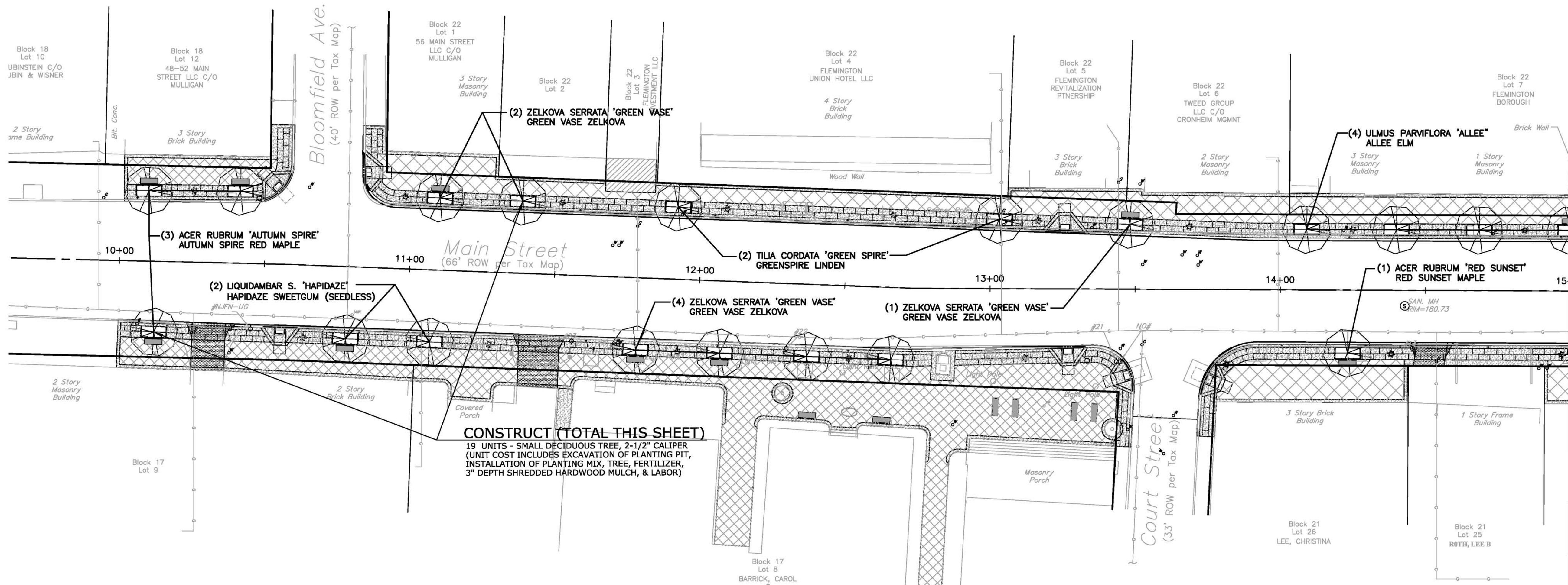
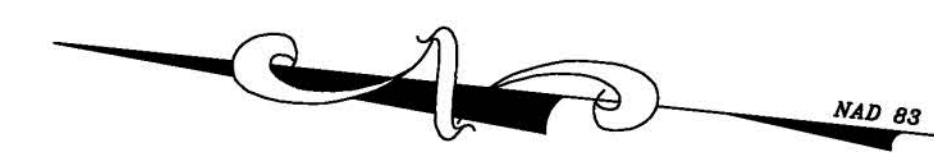
* RECEPTACLE CIRCUIT SHALL BE CONTROLLED BY RELAY PANEL.
 ** STREET LIGHTING SHALL BE CONTROLLED BY RELAY PANEL. PROVIDE ADDITIONAL UN-SWITCHED HOT LEGS WITH LIGHTING CIRCUIT FOR HI-LOW CONTROL.

DESIGNATION: S2CC		VOLTAGE: 240/120V, 1ø, 3W		LOCATION: CONTROL CABINET											
MAINS: 100A M.C.B.		FED BY: UTILITY													
TYPE: GENERAL		MINIMUM O.C. DEVICE: 10,000 AC RMS SYM													
O.C. DEVICE: M.C.B. SERVICE ENTRANCE RATED		INTERRUPTING RATING:													
MOUNTING: SURFACE															
Description	COND.	GND.	WIRE NO. SIZE	AMPS	POLES	CKT.	KVA # A	KVA # C	CKT.	POLES	AMPS	WIRE SIZE NO.	GND.	COND.	Description
* RECEPTS: NORTH	1 1/2"	8	2 8	20	1	1	0.72	0.23	2	2	20	8 3	8	1 1/2"	STREET LIGHTS: NORTH
* RECEPTS: SOUTH	1 1/2"	8	2 8	20	1	3		0.72	0.23	4					
LIGHTING CONTROL PANEL	3/4"	12	2 12	20	1	5	0.50	0.30	6	2	20	8 3	8	1 1/2"	STREET LIGHTS: SOUTH
RECEPT: CABINET MID	3/4"	12	2 12	20	1	7		0.18	0.30	8					
CABINET STRIP HEATER	3/4"	12	2 12	20	1	9	0.80		10	2	20				SPARE
SPARE									12						SPARE
SPARE									13						SPARE
SPARE									14	2	20				SPARE
SPARE									15						SPARE
SPARE									17						SPARE
TOTAL KVA/#							2.55	1.75							
TOTAL KVA							4.30								
TOTAL AMPS							17.92								

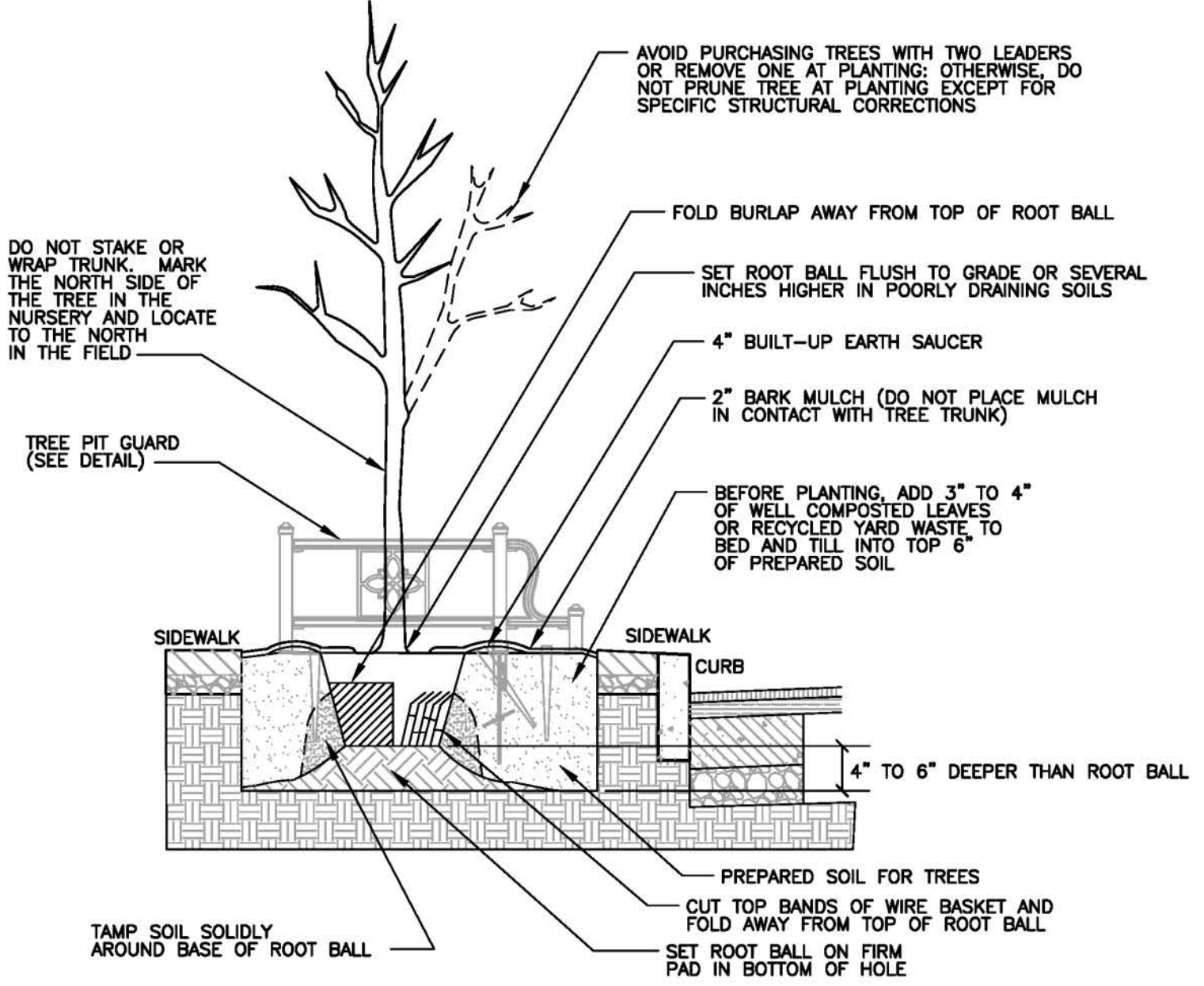
* RECEPTACLE CIRCUIT SHALL BE CONTROLLED BY RELAY PANEL.
 ** STREET LIGHTING SHALL BE CONTROLLED BY RELAY PANEL. PROVIDE ADDITIONAL UN-SWITCHED HOT LEGS WITH LIGHTING CIRCUIT FOR HI-LOW CONTROL.

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GPI Certificate of Authorization: 24GA27959500
 John Murphy
 04/24/2017
 John D. Murphy, PE
 N.J. Professional Engineer No. 24GE0422200



CONSTRUCT (TOTAL THIS SHEET)
 19 UNITS - SMALL DECIDUOUS TREE, 2-1/2" CALIPER
 (UNIT COST INCLUDES EXCAVATION OF PLANTING PIT, INSTALLATION OF PLANTING MIX, TREE, FERTILIZER, 3" DEPTH SHREDDED HARDWOOD MULCH, & LABOR)



- NOTES:**
- FOR CONTAINER GROWN TREES, USE FINGERS OR SMALL HAND TOOLS TO PULL THE ROOTS OUT OF THE OUTER LAYER OF POTTING SOIL; THEN CUT OR PULL APART ANY ROOTS CIRCLING THE PERIMETER OF THE CONTAINER.
 - INCORPORATE COMMERCIALY PREPARED MYCORRHIZA SPORES IN THE SOIL IMMEDIATELY AROUND THE ROOT BALL AT RATES SPECIFIED BY THE MANUFACTURER.
 - CONFIRM THAT WATER DRAINS OUT OF THE SOIL. NOTIFY ENGINEER IF THE EXISTING SOIL DOES NOT DRAIN SUFFICIENTLY. DESIGN ALTERNATIVE DRAINAGE SYSTEMS AS REQUIRED AND APPROVED BY ENGINEER.
 - THOROUGHLY SOAK THE TREE ROOT BALL AND ADJACENT PREPARED SOIL SEVERAL TIMES DURING THE FIRST MONTH AFTER PLANTING AND REGULARLY THROUGHOUT THE FOLLOWING TWO SUMMERS.
 - THE PLANTING PROCESS IS SIMILAR FOR DECIDUOUS AND EVER-GREEN TREES.

PLANTING NOTES:

- TREES AND SHRUBS SHALL BEAR SAME RELATIONSHIP TO FINISH GRADE AS THEY BORE TO THE GRADE IN THE NURSERY OR CONTAINER.
- CUT AND REMOVE BURLAP FROM TOP 1/3 OF ROOT BALL.
- ALL PLANT MATERIAL TO BE APPROVED AND TAGGED AT THE NURSERY SOURCE.
- FINAL LOCATION OF ALL TREES AND SHRUBS TO BE DETERMINED IN THE FIELD BY THE ENGINEER BEFORE PLANTING.
- ALL TREES AND ENTIRE SHRUB BEDS TO RECEIVE A LAYER OF SHREDDED HARDWOOD BARK MULCH TO A DEPTH OF 3" (MAX.).
- MULCH SHALL NOT TOUCH BARK OF TREES OR SHRUBS.
- LAWN SEED MIX SHALL BE AS FOLLOWS:
 SUMMER STRESS MIXTURE (OR APPROVED EQUAL)
 90% REBEL II, REBEL JR., OR TRIBUTE TALL FESCUE
 10% GEORGETOWN, RAM I, NASSAU, OR BARON KENTUCKY BLUEGRASS
 SEEDING DATES: 4/1-5/31 & 8/16-10/15 (OR AS APPROVED)
 SEEDING RATES: 220 LBS. PER ACRE
- ALL DISTURBED AREAS NOT SHOWN AS PLANTED OR LAWN AREA ARE TO BE TOPSOILED AND SEEDED OR RETURNED TO THEIR ORIGINAL CONDITION BEFORE DISTURBANCE AS DIRECTED BY THE LANDSCAPE ARCHITECT.
- ALL AREAS NOT RELATED TO DEVELOPMENT ARE TO REMAIN IN THEIR NATURAL STATE.
 ALL LANDSCAPING SHALL BE GUARANTEED FOR A PERIOD OF ONE YEAR FROM DATE OF ACCEPTANCE BY THE OWNER. LANDSCAPING THAT DOES NOT SURVIVE, OR IN THE OPINION OF THE ENGINEER IS NOT THRIVING, SHALL BE REPLACED WITH THE SAME SPECIES OR APPROVED EQUAL AT NO COST TO THE OWNER.
- ALL PLANT MATERIAL SHALL MEET THE MINIMUM REQUIREMENTS FOR NURSERY STOCK AS DEFINED IN "AMERICAN STANDARD FOR NURSERY STOCK" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN, LATEST EDITION.
- COLLECTED PLANT MATERIAL WILL NOT BE ACCEPTED

TABLE OF QUANTITIES

ITEM NO.	DESCRIPTION	UNIT	QUANTITY
46	SMALL DECIDUOUS TREE, 2-2 1/2" CALIPER	UNITS	19

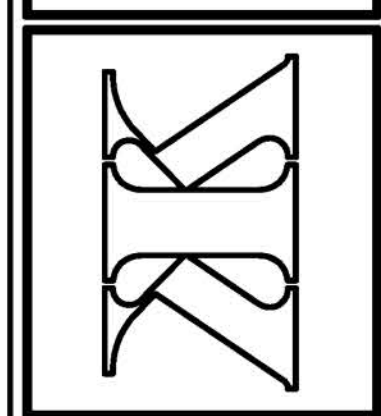
TREE PLANTING DETAIL (BALLED AND BURLAPPED PLANTS)
 N.T.S.

MATCHLINE STATION 15+00 - SEE SHEET 18 OF 31

NO.	REVISIONS	DATE
6		
5		
4	NO REVISION THIS SHEET	6/13/18
3	PER SHPO: REMOVE TREES @ UNION HOTEL	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18
1	PER MADD COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17
 SCALE: 1" = 20'
 PROJ.: 2016020
 FLD. BK.: 75-6
 PAGE: 28
 CHKD.:
 0 10 20 40 80

Keller & Kirkpatrick Inc.
 2260 Main Street, Suite 204, Morris Plains, NJ 07950 973-377-6600
 CONSULTING ENGINEERS
 LAND SURVEYORS
 LANDSCAPE ARCHITECTS
 PLANNERS
 ANDREW CANGIANO, Professional Engineer, Lic. 43111

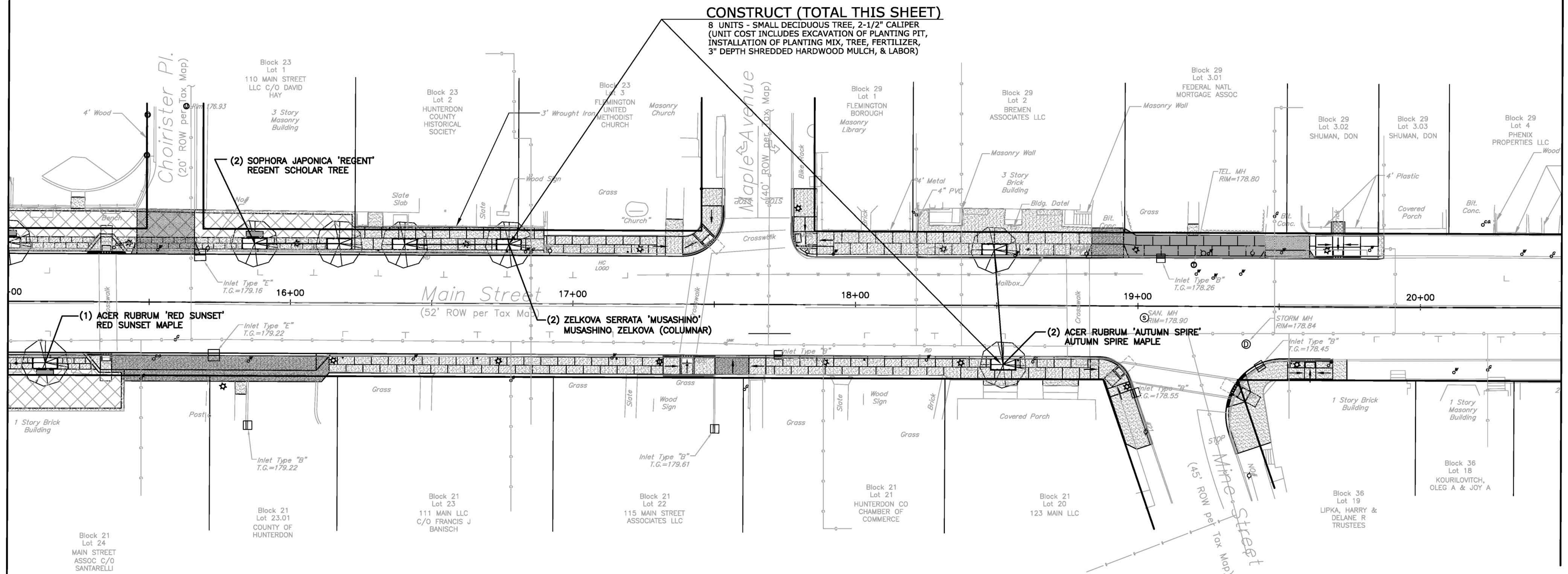


MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
 PLANTING PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
 17
 OF 31

SEE SHEET 2 OF 31 FOR GENERAL NOTES

MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31



MATCHLINE STATION 20+50 - THIS SHEET

MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31

MATCHLINE STATION 20+50 - THIS SHEET

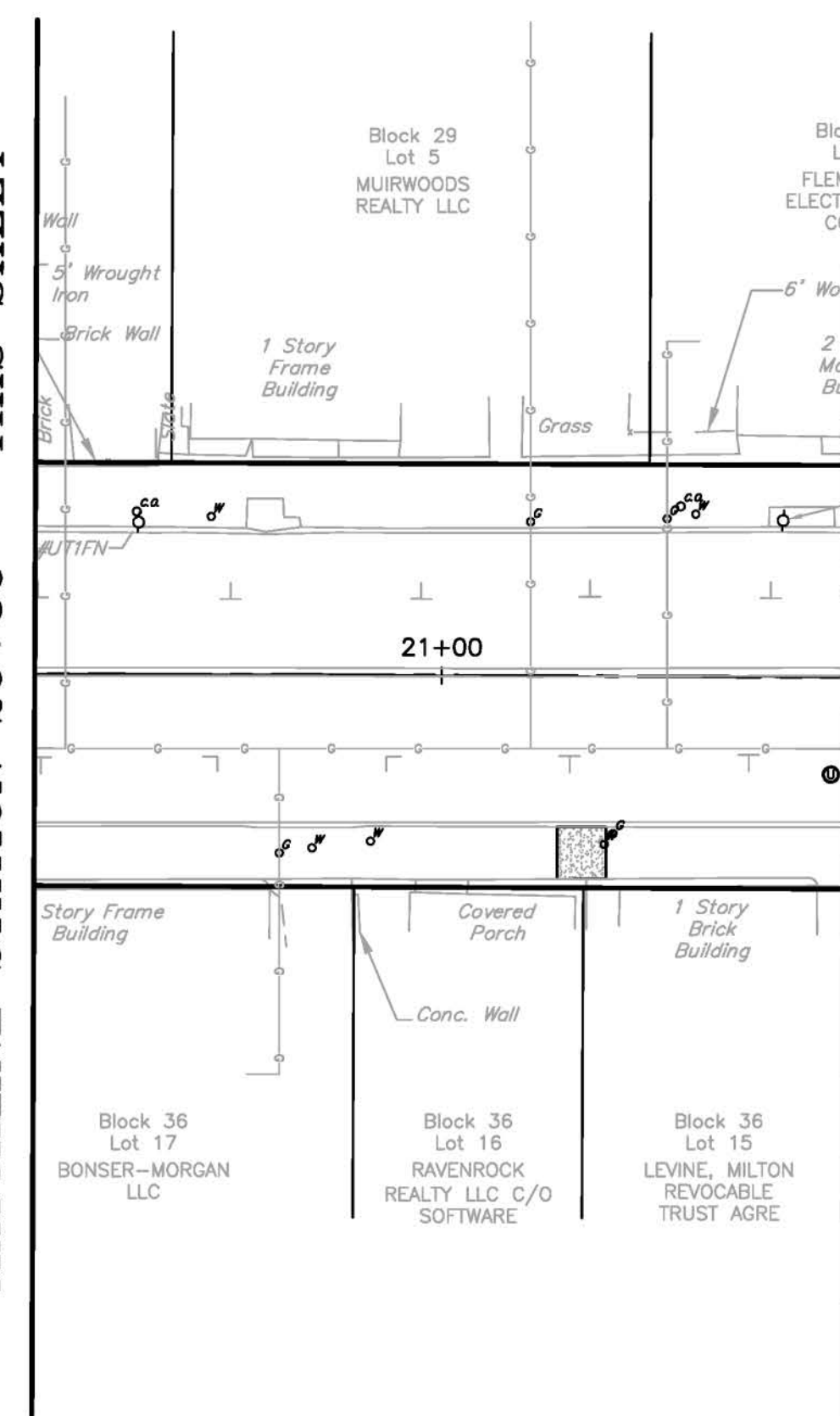


TABLE OF QUANTITIES		
ITEM NO.	DESCRIPTION	QUANTITY
46	SMALL DECIDUOUS TREE, 2-2 1/2" CALIPER	7

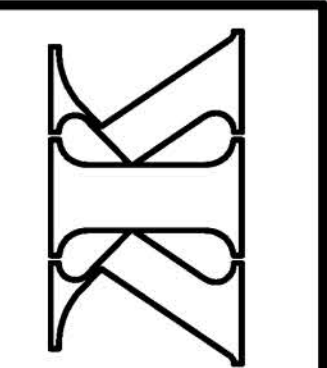
SEE SHEET 2 OF 31 FOR GENERAL NOTES

NO.	REVISIONS	DATE
6		
5		
4	NO REVISION THIS SHEET	6/13/18
3	PER SHPO: REMOVE TREES @ UNION HOTEL	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18
1	PER MDDOT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17	SCALE: 1" = 20'	F.L.D. BK.: 75-6
PROJ.: 2016020	CHKD.:	PAGE: 28

Keller & Kirkpatrick Inc.
 501 Gibraltar Drive, Suite 24, Morris Plains, NJ 07960 973-377-6600
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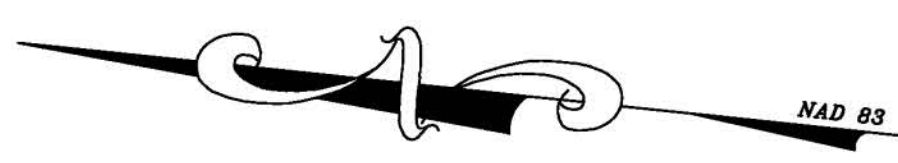
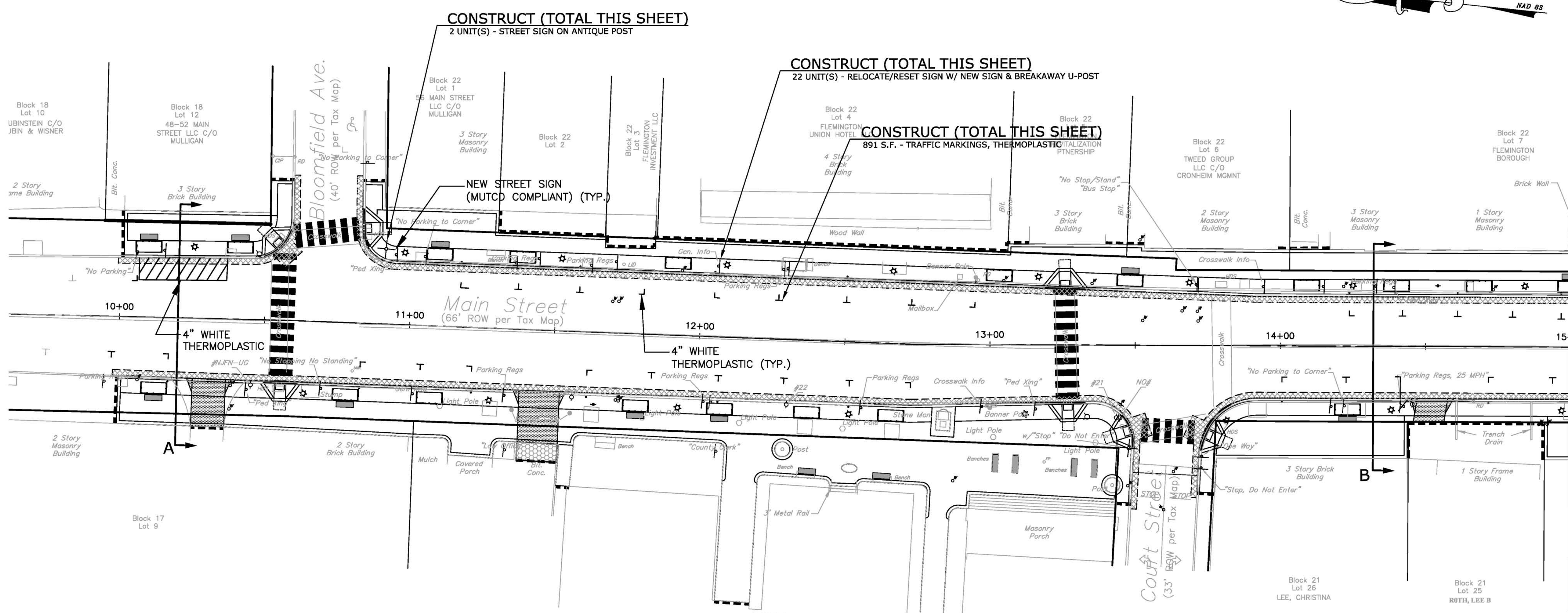
Andrew Cangiano
 ANDREW CANGIANO, Professional Engineer, Lic. 43111



MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
PLANTING PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

SHEET No.
18
 OF **31**

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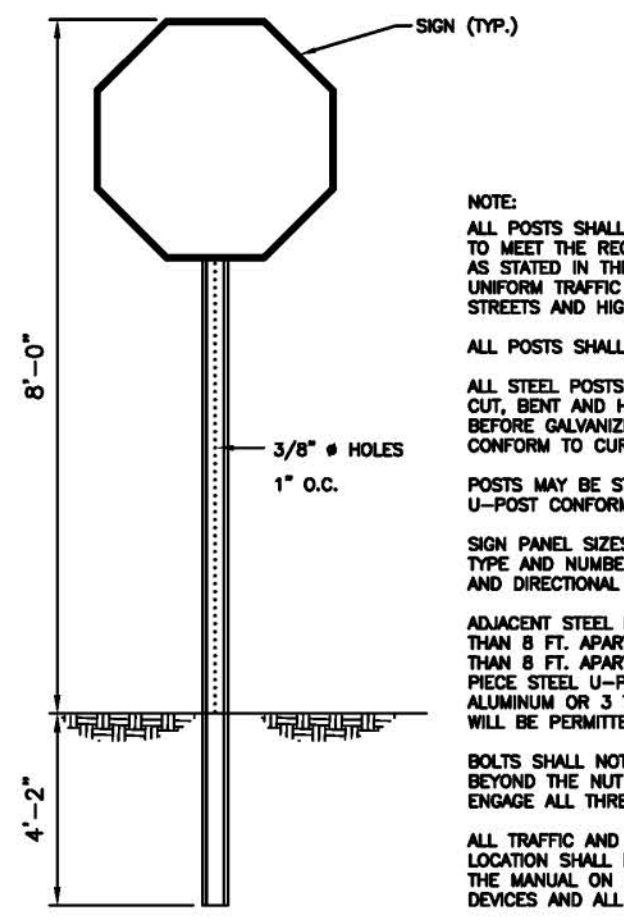


MATCHLINE STATION 15+00 - SEE SHEET 20 OF 31

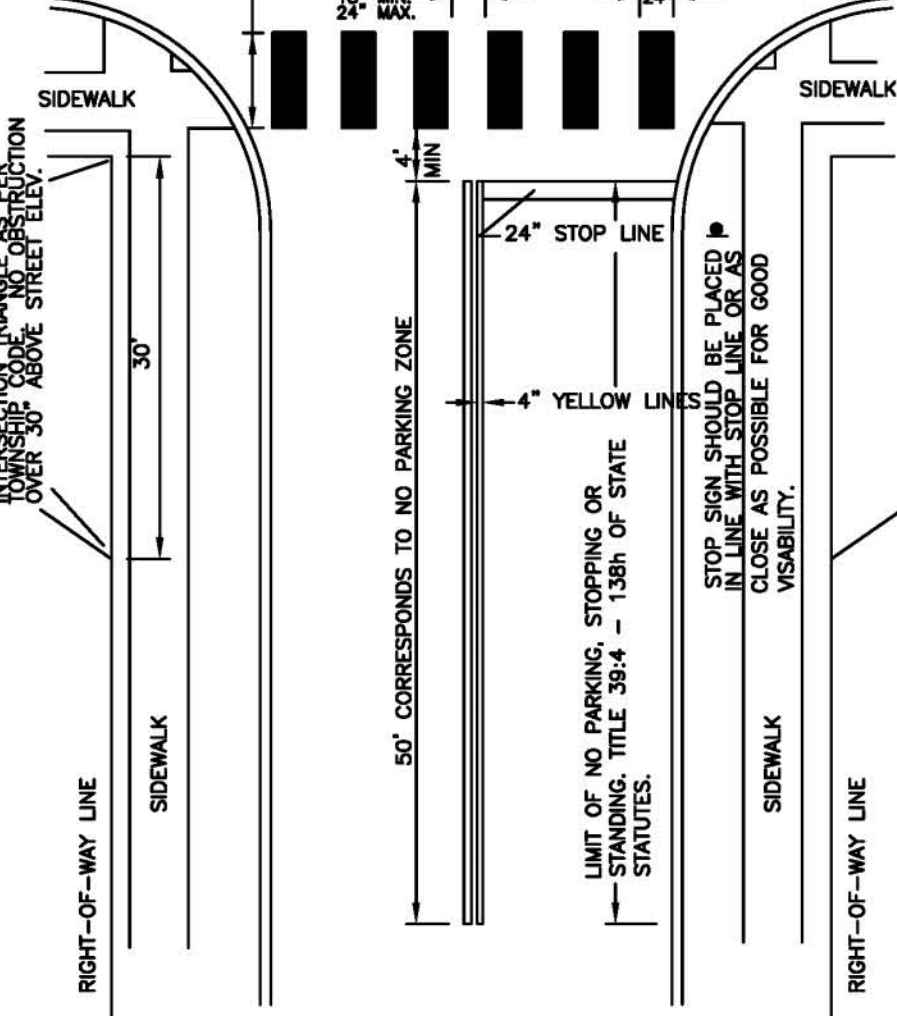
NOTE:
ALL EXISTING ROADWAY SIGNS SHALL BE REMOVED DURING SITE PREPARATION AND ARE TO BE DELIVERED TO THE BOROUGH AS DIRECTED.

NOTE: THIS CROSSWALK MARKING APPLIES FOR NEW MARKINGS ON RESURFACED ROADWAYS ONLY. CROSSWALKS MARKINGS ON EXISTING ROADWAYS NOT SLATED TO BE RESURFACED SHALL BE MAINTAINED WITH EXISTING PATTERN.

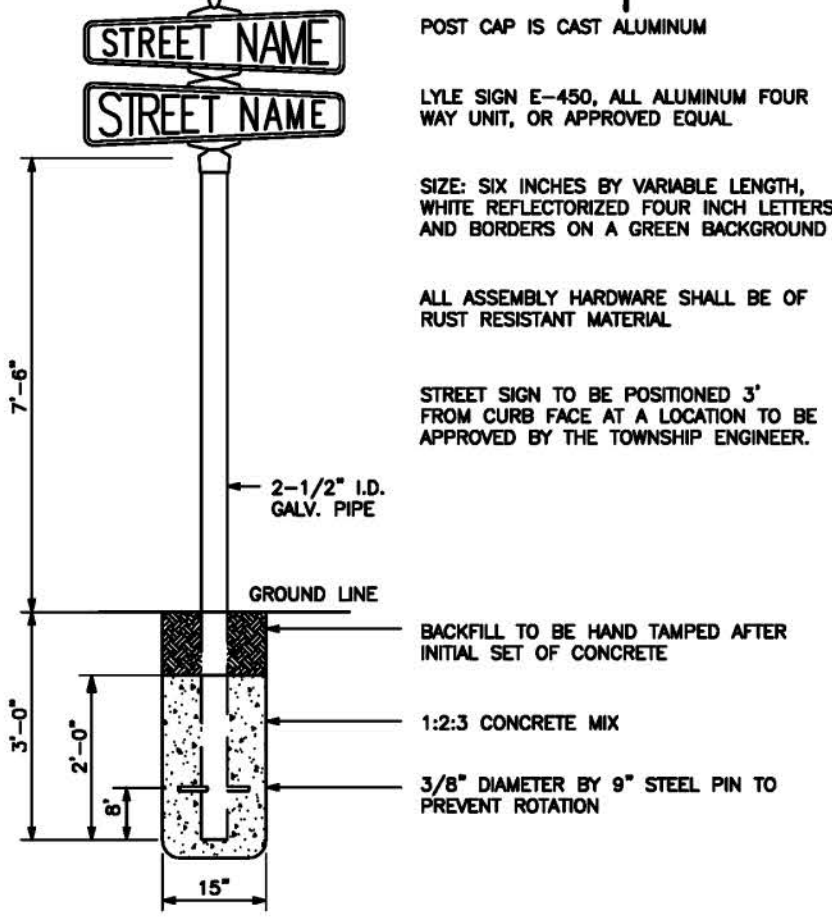
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ITEM NO.	DESCRIPTION	UNIT	QUANTITY SHEET 19
25	TRAFFIC MARKINGS, THERMOPLASTIC	SF	891
27	RELOCATE/RESET SIGN WITH NEW SIGN & BREAKAWAY U-POST	UNITS	22
28	STREET SIGN ON ANTIQUE POST	UNITS	2



SIGN DETAIL
N.T.S.



PEDESTRIAN CROSSWALK DETAIL
N.T.S.

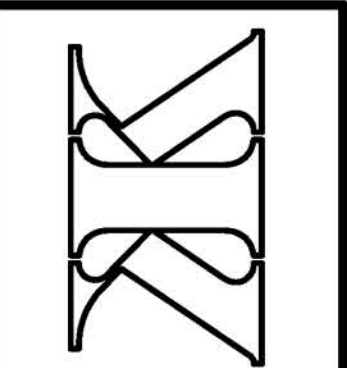


STREET SIGN
N.T.S.

NO.	REVISIONS	DATE	BY
6			
5			
4	NO REVISION THIS SHEET	6/13/18	
3	PER SHPO: REMOVE TREES @ UNION HOTEL	5/16/18	
2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18	
1	PER MDDT COMMENT RE: ROW CERTIFICATION	10/17/17	

DATE: 4/24/17
SCALE: 1" = 20'
F.L.D. BK.: 75-6
PROJ.: 2016020
PAGE: 28
CHKD.:
REVISIONS

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201 Gibraltar Drive, Suite 204, Morris Plains, NJ 07960 973-377-8600
LANDSCAPE ARCHITECTS
PLANNERS
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LAND SURVEYORS
ANDREW CANGIANO, Professional Engineer, Lic. 43111

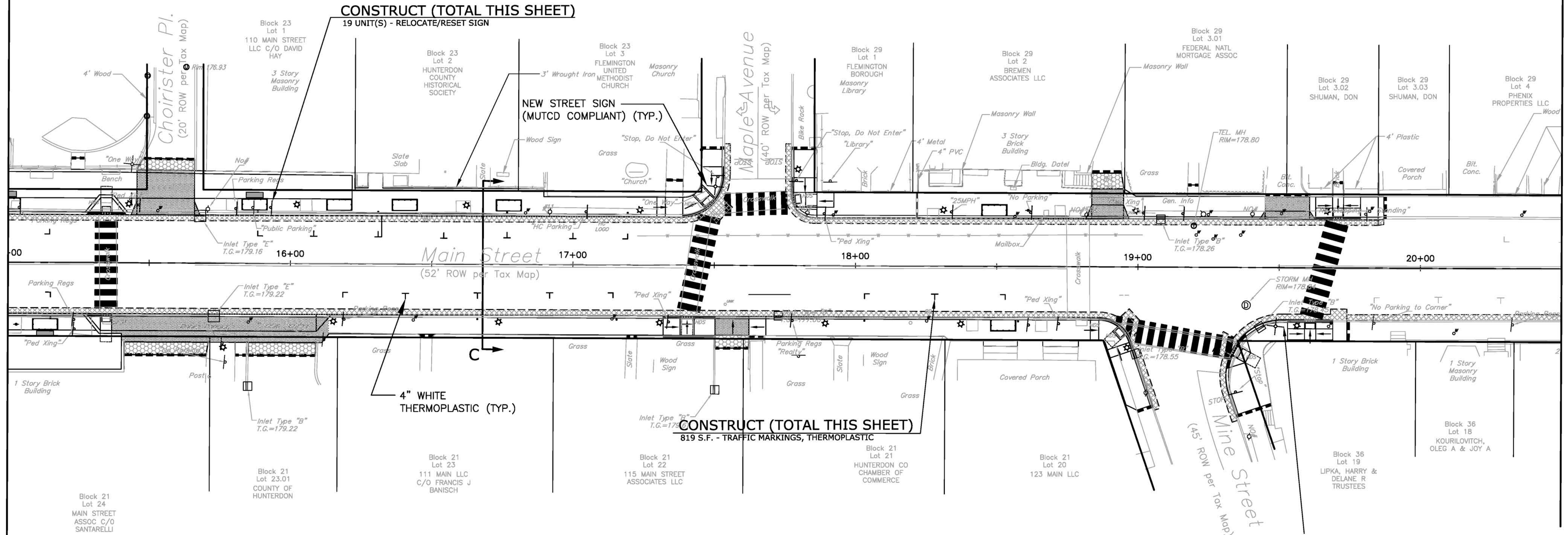


MAIN STREET SIDEWALK & STREETScape
BLOOMFIELD AVENUE TO MINE STREET
SIGNING & STRIPING PLAN
BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
FEDERAL AID PROJECT #TAP-1006(335)

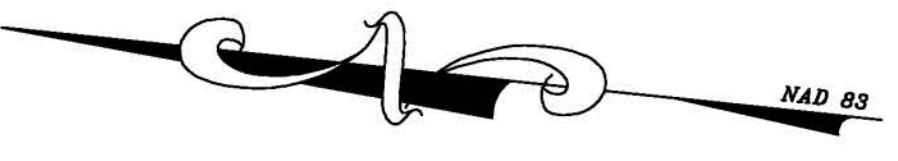
SHEET No.
19
OF
31

SEE SHEET 2 OF 31 FOR GENERAL NOTES

MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31



MATCHLINE STATION 20+50 - THIS SHEET

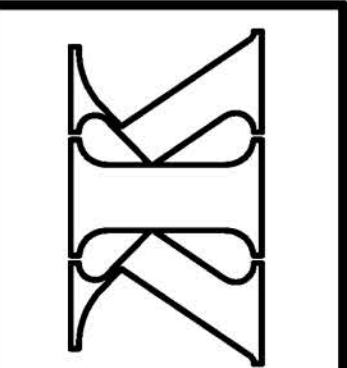


NO.	REVISIONS	DATE
6		
5		
4	NO REVISION THIS SHEET	6/13/18
3	NO REVISION THIS SHEET	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/29/18
1	PER MDDOT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17	SCALE: 1" = 20'	F.L.D. BK.: 75-6
PROJ.: 2016020	CHKD.:	PAGE: 28

Keller & Kirkpatrick Inc.
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 CONSULTING ENGINEERS
 LANDSCAPE ARCHITECTS
 PLANNERS

Andrey Gungano
 ANDREY GUNGANO, Professional Engineer, Lic. 43111



MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
SIGNING & STRIPING PLAN

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

SHEET No.
20
 OF **31**

MATCHLINE STATION 20+50 - THIS SHEET

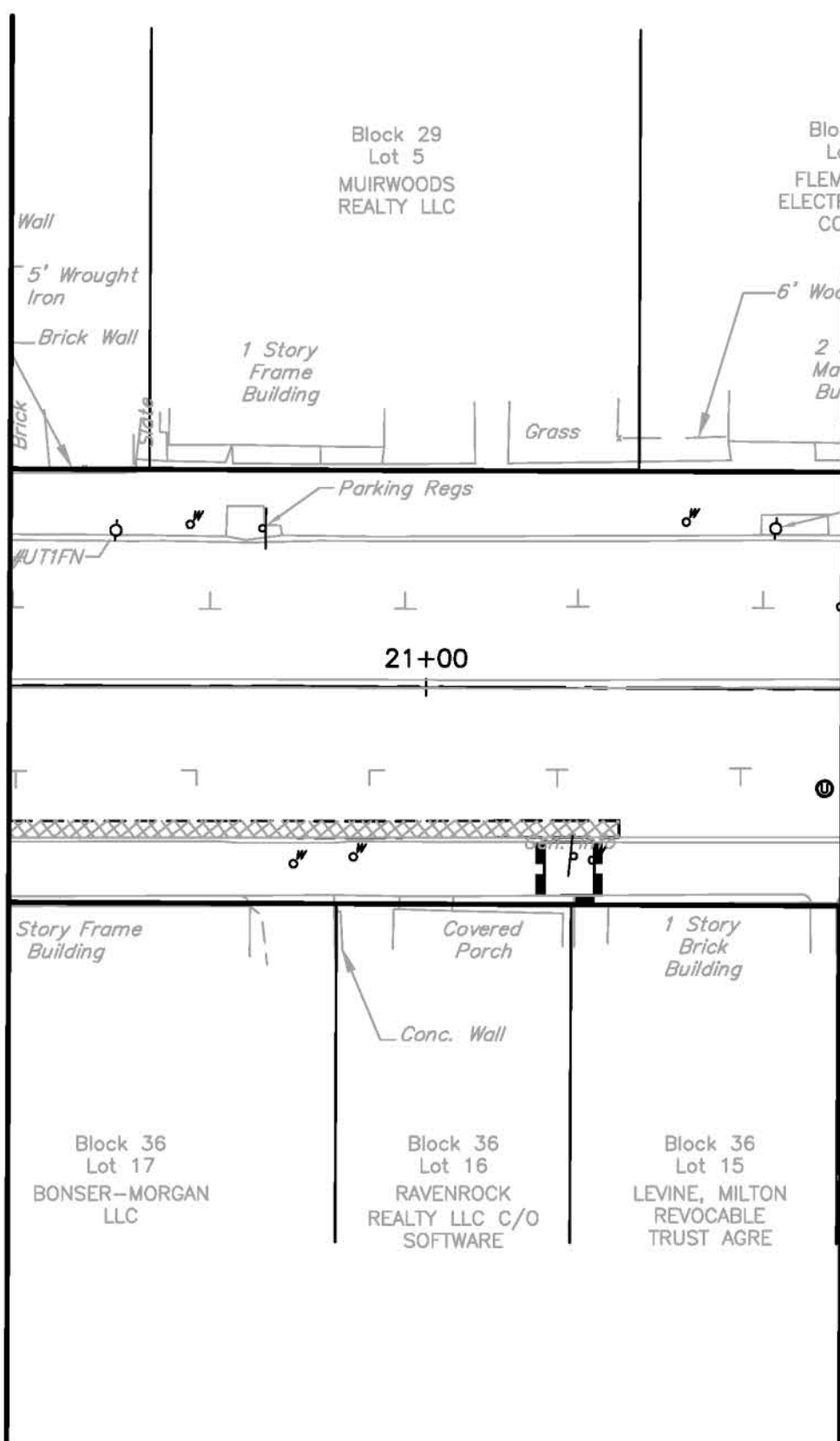
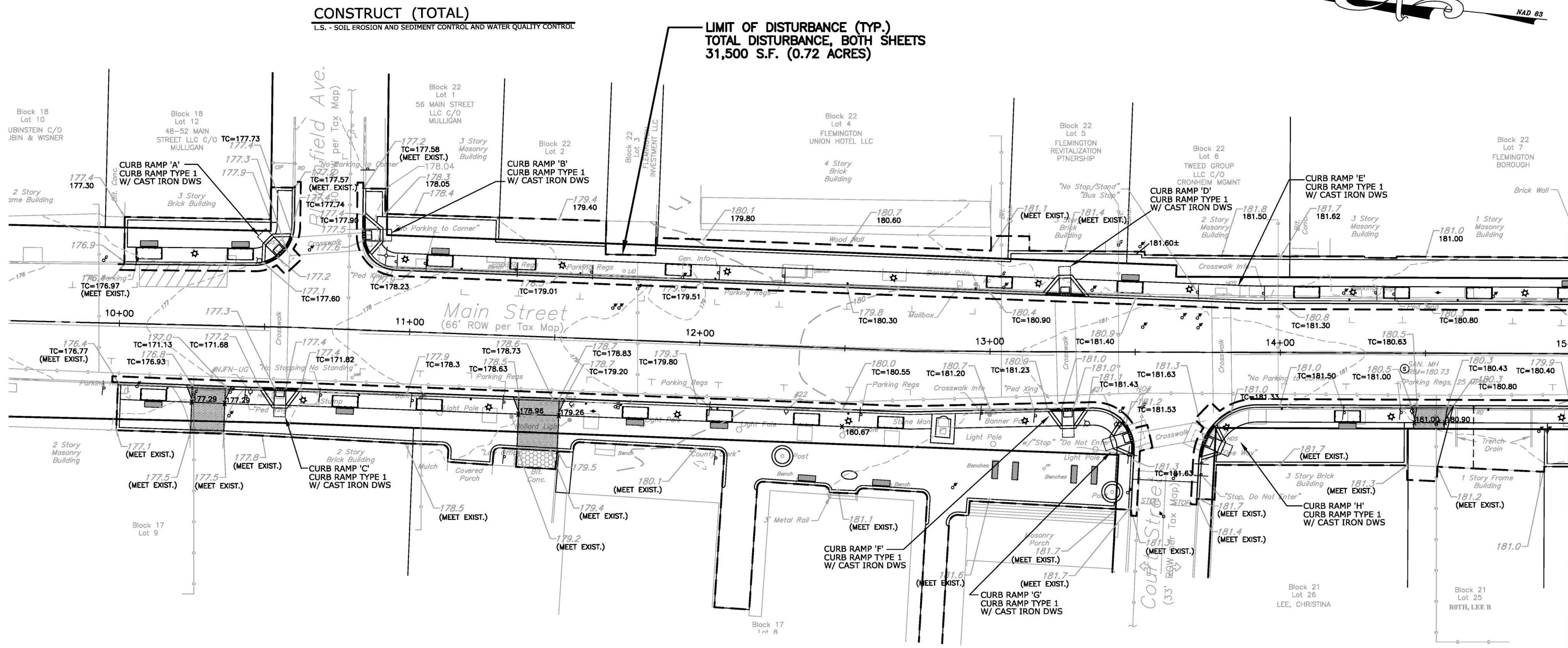


TABLE OF QUANTITIES		
ITEM NO.	DESCRIPTION	QUANTITY
25	TRAFFIC MARKINGS, THERMOPLASTIC	SF 819
27	RELOCATE/RESET SIGN WITH NEW SIGN & BREAKAWAY U-POST	UNITS 19
28	STREET SIGN ON ANTIQUE POST	UNITS 2

SEE SHEET 2 OF 31 FOR GENERAL NOTES

W:\2016020\Draw\Drawings\2016020 SIGN_STRIPING.dwg, Layout 2, 6/13/2018 1:48:37 PM, psmalida, 11



CONSTRUCT (TOTAL)
 L.S. - SOIL EROSION AND SEDIMENT CONTROL AND WATER QUALITY CONTROL

LIMIT OF DISTURBANCE (TYP.)
 TOTAL DISTURBANCE, BOTH SHEETS
 31,500 S.F. (0.72 ACRES)

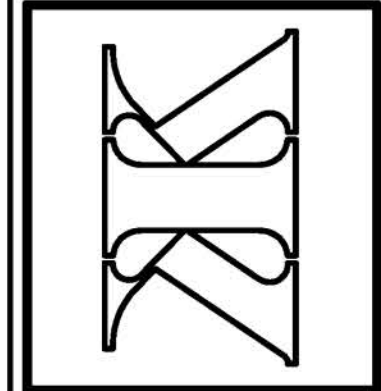
MATCHLINE STATION 15+00 - SEE SHEET 22 OF 31

NO.	REVISIONS	DATE
6		
5		
4	NO REVISION THIS SHEET	6/13/18
3	PER SHPO: REMOVE TREES @ UNION HOTEL	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/29/18
1	PER MDDT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17	SCALE: 1" = 20'	F.L.D. BK.: 75-6
PROJ.: 2016020	CHKD.:	PAGE: 28

Keller & Kirkpatrick Inc.
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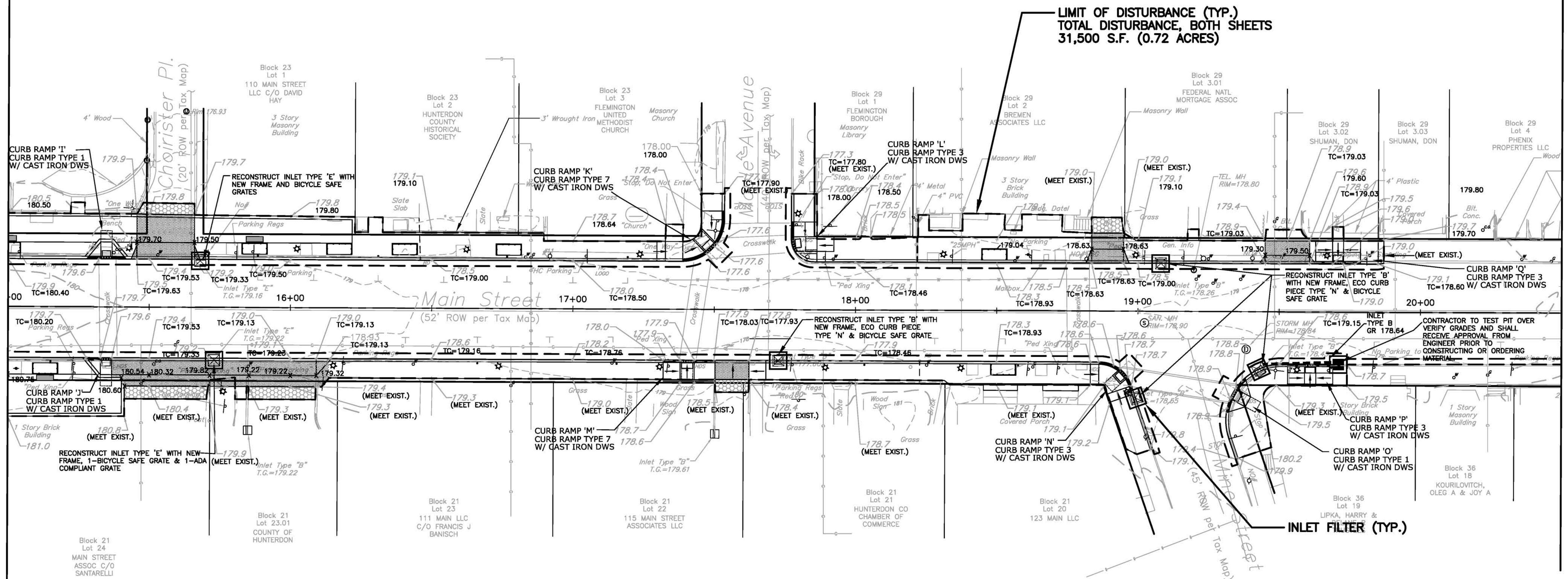
MAIN STREET SIDEWALK & STREETScape
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SOIL EROSION & SEDIMENT CONTROL PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

SHEET No.
21
 OF **31**

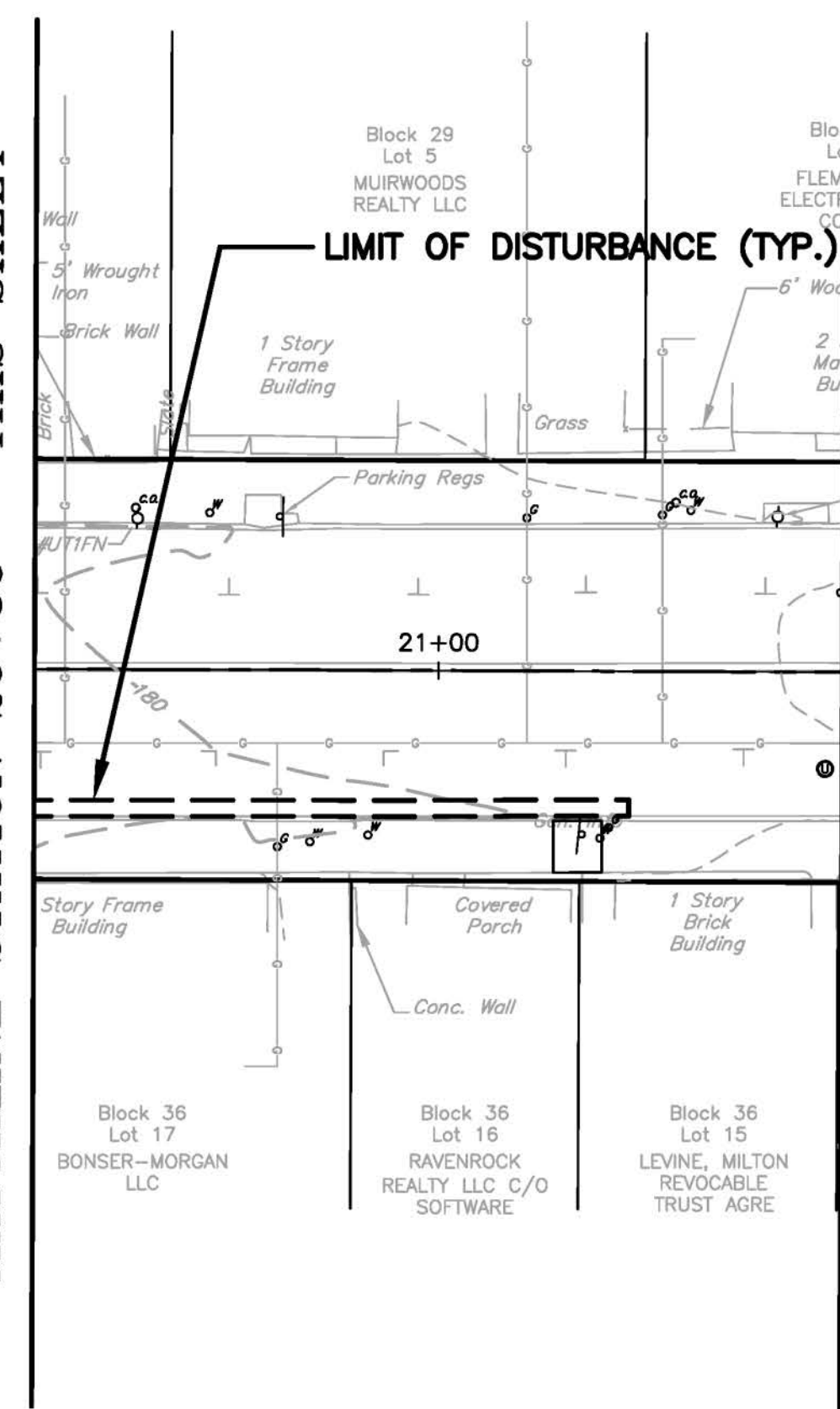
SEE SHEET 2 OF 31 FOR GENERAL NOTES

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MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31



MATCHLINE STATION 20+50 - THIS SHEET



SEE SHEET 2 OF 31 FOR GENERAL NOTES

6				
5				
4	NO REVISION THIS SHEET	6/13/18		
3	NO REVISION THIS SHEET	5/16/18		
2	PER SHPO COMMENT RE: CONCRETE CURB	3/28/18		
1	PER MDOT COMMENT RE: ROW CERTIFICATION	10/17/17		

REVISIONS

DATE: 4/24/17 SCALE: 1" = 20' FLD. BK.: 75-6

PROJ.: 2016020 CHKD.: PAGE: 28

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 501 Gibraltar Drive, Suite 204, Morris Plains, NJ 07960 973-377-8600
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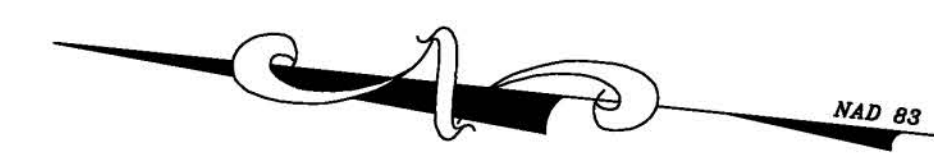
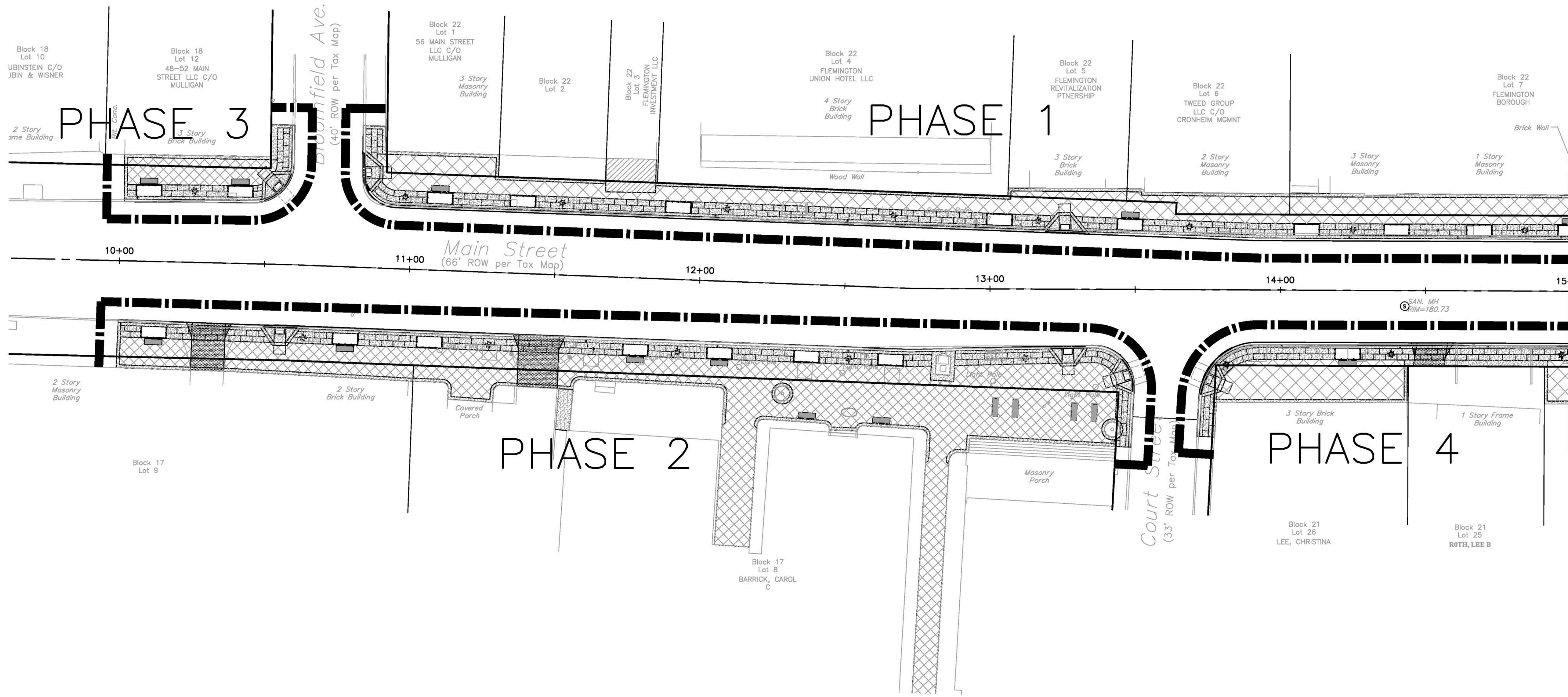
LANDSCAPE ARCHITECTS
 PLANNERS

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MAIN STREET SIDEWALK & STREETScape
BLOOMFIELD AVENUE TO MINE STREET
SOIL EROSION & SEDIMENT
CONTROL PLAN

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

SHEET No.
22
 OF **31**



MATCHLINE STATION 15+00 - SEE SHEET 24 OF 31

NOTES:

1. CONTRACTOR TO COORDINATE ALL PHASING WITH BOROUGH OF FLEMINGTON, RESIDENT ENGINEER, AND PROPERTY OWNERS/STORE OPERATORS.
2. ACCESS TO BUSINESSES, RESIDENCES, ETC. TO BE MAINTAINED AT ALL TIMES UNLESS PRIOR ARRANGEMENTS HAVE BEEN MADE AND AGREED UPON BY ALL PARTIES.
3. CONTRACTOR TO PROVIDE APPROPRIATE SIGNAGE TO ADVISE PEDESTRIANS/VEHICULAR TRAFFIC OF CLOSED SIDEWALKS/DRIVEWAYS AND SAFE DETOURS AROUND SIDEWALKS/DRIVEWAY CLOSINGS.
4. CONTRACTOR TO PREPARE AND SUBMIT A TRAFFIC/PEDESTRIAN CONTROL PLAN TO THE BOROUGH AND THE RESIDENT ENGINEER PRIOR TO THE START OF ANY DEMOLITION.

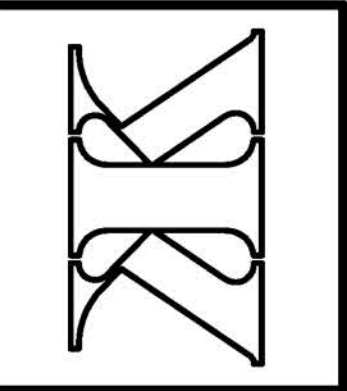
SEE SHEET 2 OF 31 FOR GENERAL NOTES

NO.	REVISIONS	DATE
6		
5		
4	NO REVISION THIS SHEET	6/13/18
3	PER SHPO: REMOVE TREES @ UNION HOTEL	5/16/18
2	PER SHPO COMMENT RE: CONCRETE CURB	3/29/18
1	PER MDDOT COMMENT RE: ROW CERTIFICATION	10/17/17

DATE: 4/24/17	SCALE: 1" = 20'	F.L.D. BK.: 75-6
PROJ.: 2016020	CHKD.:	PAGE: 28

Certificate of Authorization #22600027400
 SOI Gibraltar Drive, Suite 24, Morris Plains, NJ 07960 973-377-6600
Keller & Kirkpatrick Inc.
 CONSULTING ENGINEERS
 LAND SURVEYORS
 LANDSCAPE ARCHITECTS
 PLANNERS

 ANDREW CANGIANO, Professional Engineer, Lic. 4311

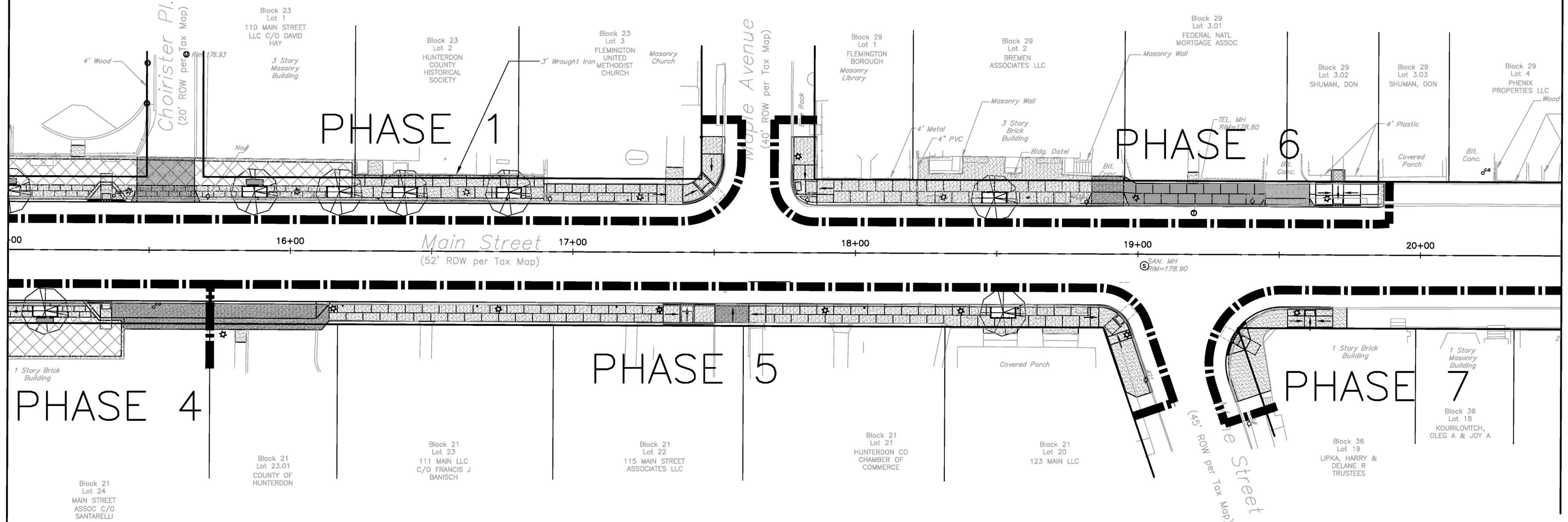


MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
PHASING PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
23
 OF
31

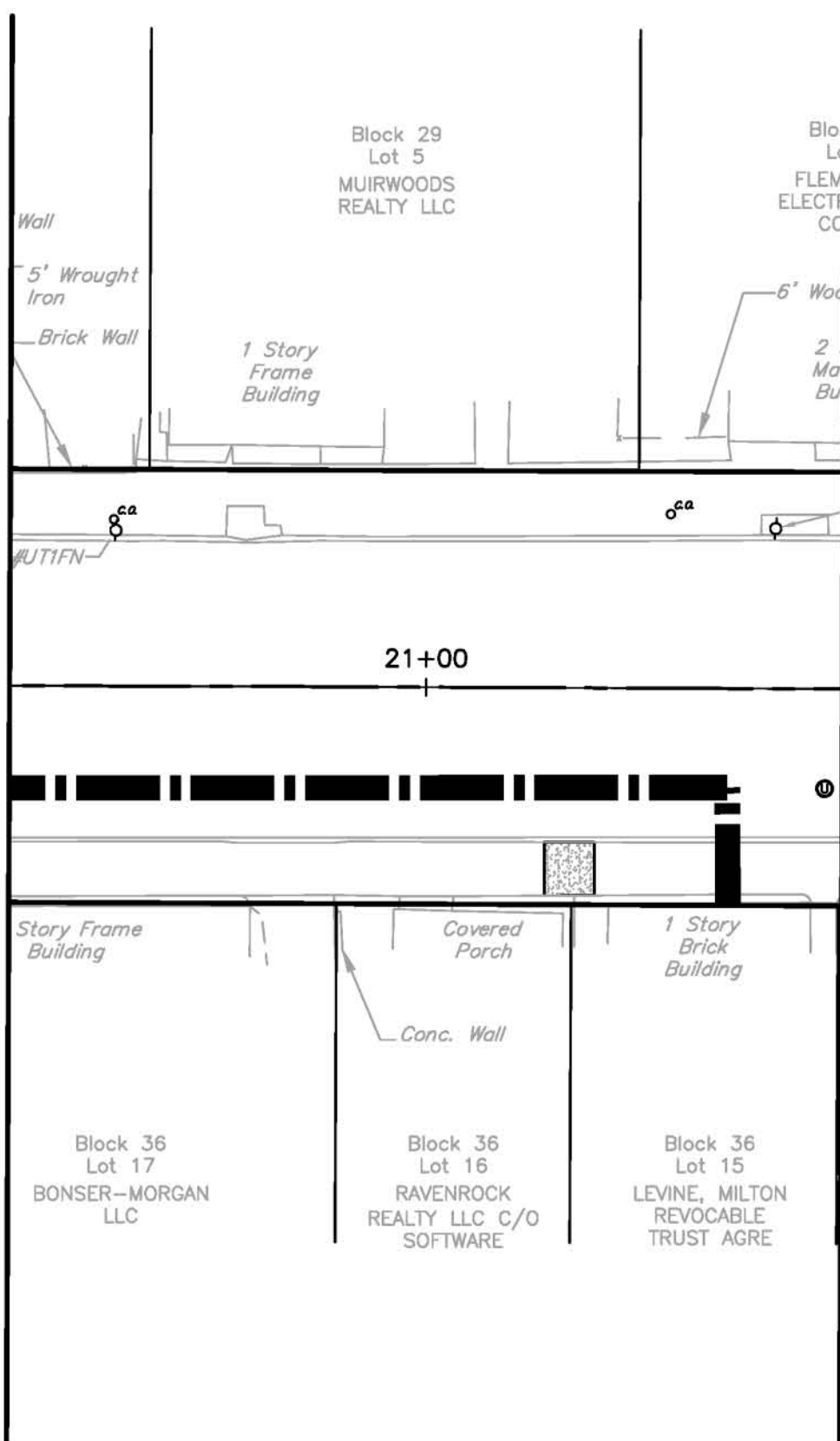
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MATCHLINE STATION 15+00 - SEE SHEET 8 OF 31



MATCHLINE STATION 20+50 - THIS SHEET

MATCHLINE STATION 20+50 - THIS SHEET



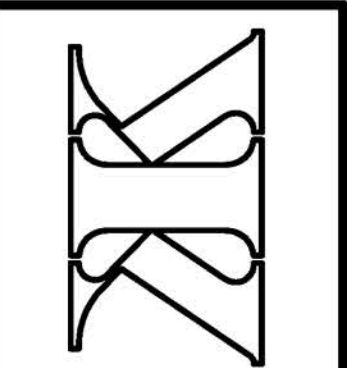
SEE SHEET 2 OF 31 FOR GENERAL NOTES

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PROJ.: 2016020	CHKD.:	PAGE: 28

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Andrey Gagliano
 ANDREY GAGLIANO, Professional Engineer, Lic. 43111



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PHASING PLAN
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
24
 OF
31

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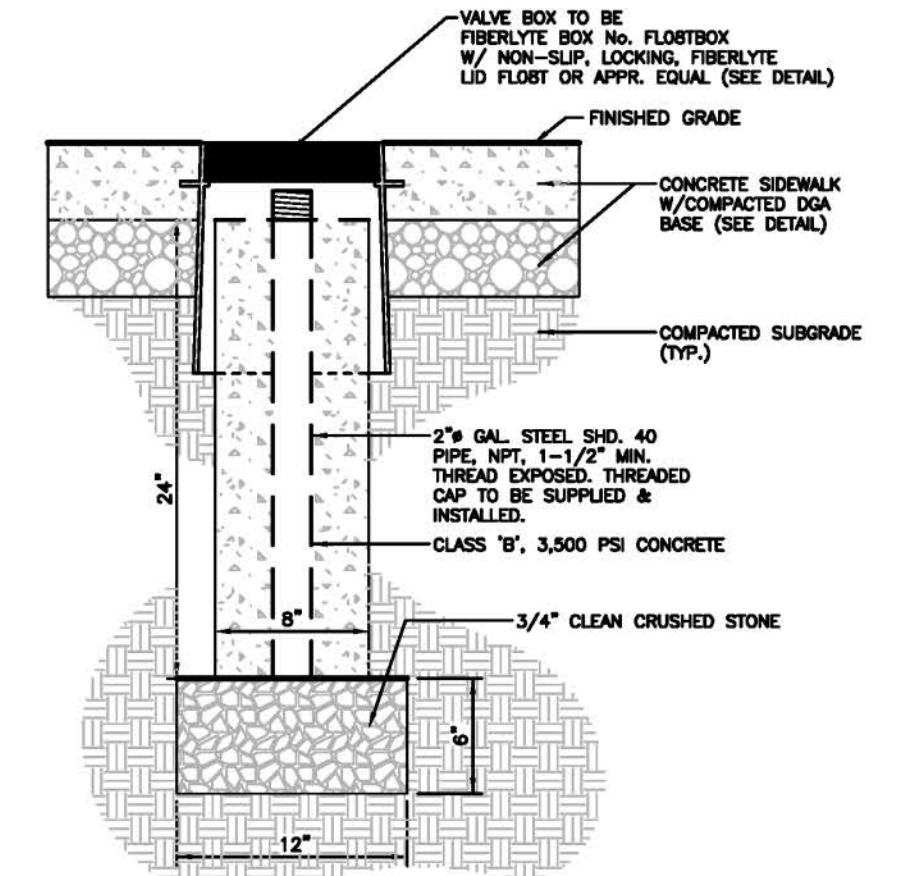
BENCH DETAIL
 CONTRACTOR TO SUPPLY AND INSTALL 6'-0" BENCH MODEL # CS-10 (6') AS MANUFACTURED BY*:
 VICTOR STANLEY, BASIS OF DESIGN MODEL #CS-10 SERIES (6') (1-800-368-2573)
 FAIRWEATHER MODEL # TD-3 (6'/1PE) (1-800-323-1798)
 DUMOR MODEL #493 (6') (1-800-598-4018)
 *OR APPROVED EQUAL
 ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE

BENCH DETAIL
 Victory Stanley Classic Collection Bench CS-10 (6') or Approved Equal
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER N.T.S.

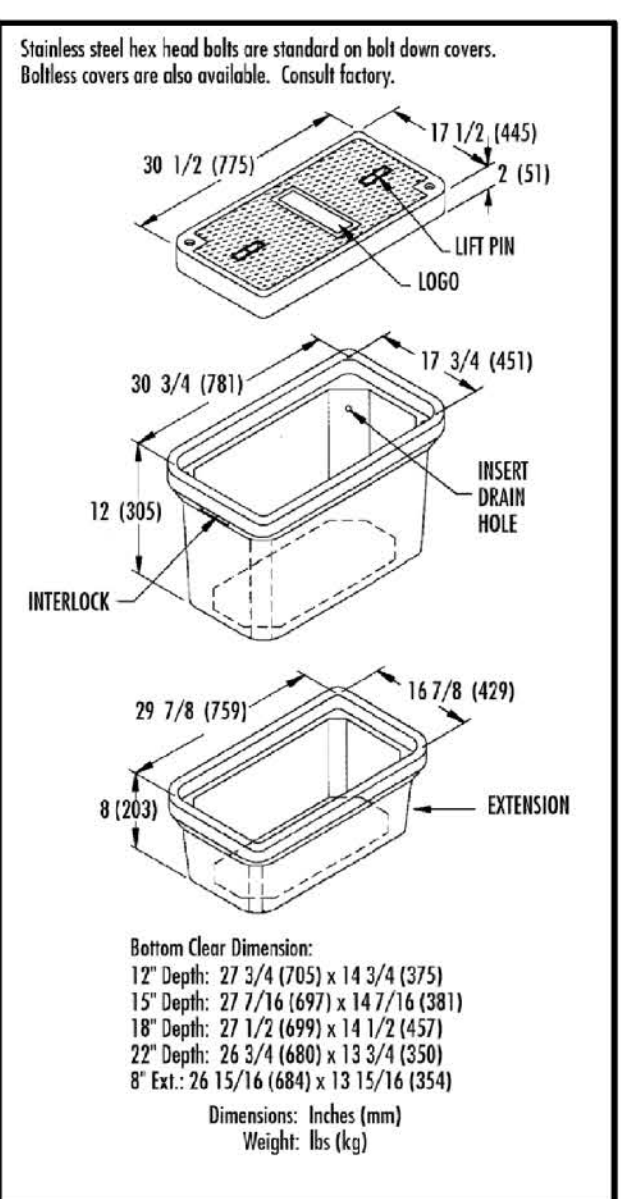


TRASH/RECYCLE STATION DETAIL
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER
 CONTRACTOR TO SUPPLY AND INSTALL TRASH/RECYCLE RECEPTACLE MODEL #S-42 AS MANUFACTURED BY*:
 VICTOR STANLEY, BASIS OF DESIGN MODEL #S-42 (1-800-368-2573)
 FAIRWEATHER MODEL #TR-12 (1-800-323-1798)
 DUMOR MODEL #157 (1-800-598-4018)
 *OR APPROVED EQUAL
 ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE

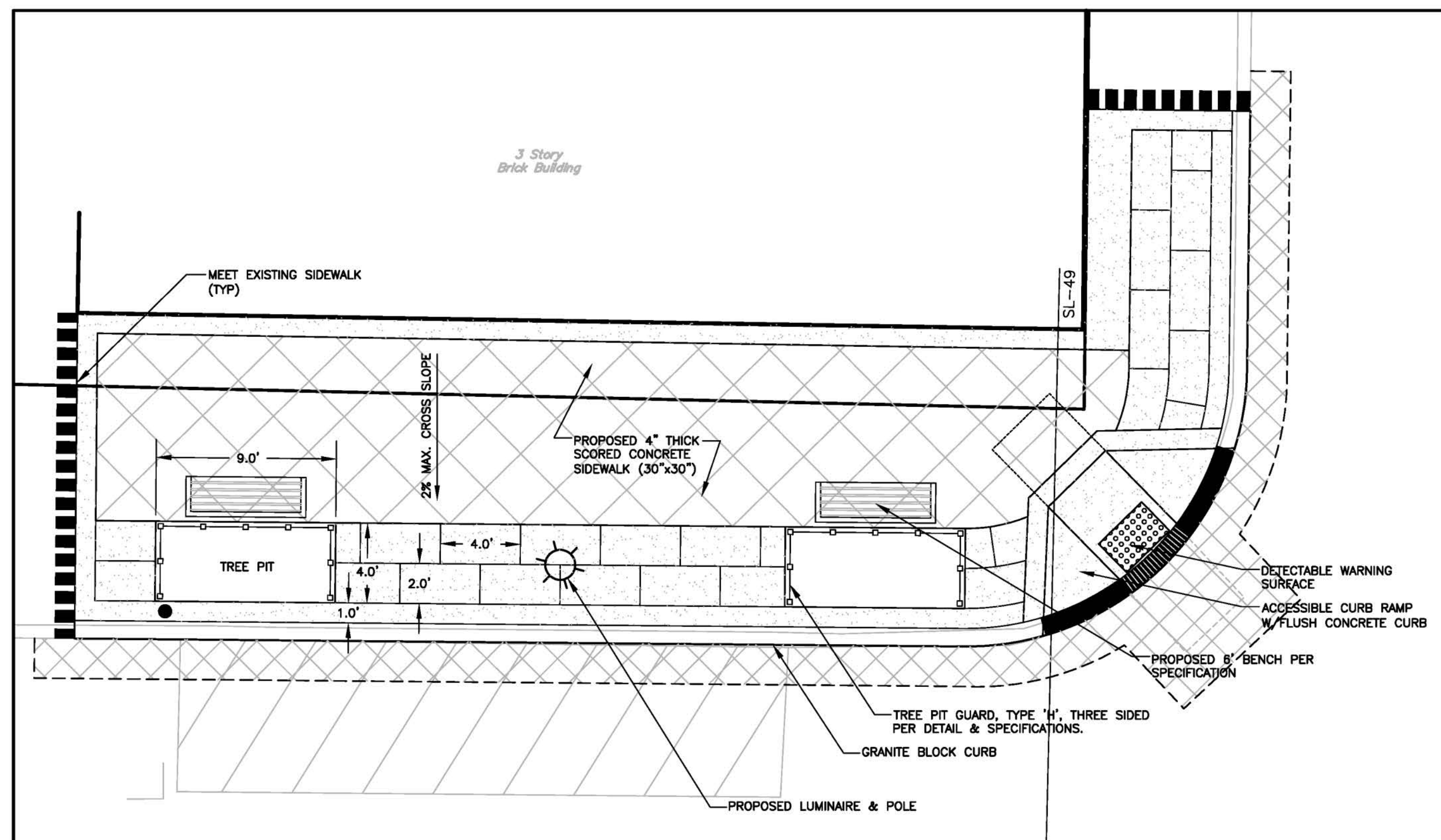
TRASH/RECYCLE STATION DETAIL
 Victory Stanley Ironites S-42 or Approved Equal
 TYPE AND QUANTITY OF TRASH AND RECYCLE CONTAINERS SHALL BE PER SPECIFICATIONS FOR LID TYPE, DECALS, PLAQUES, COLOR ETC. N.T.S.



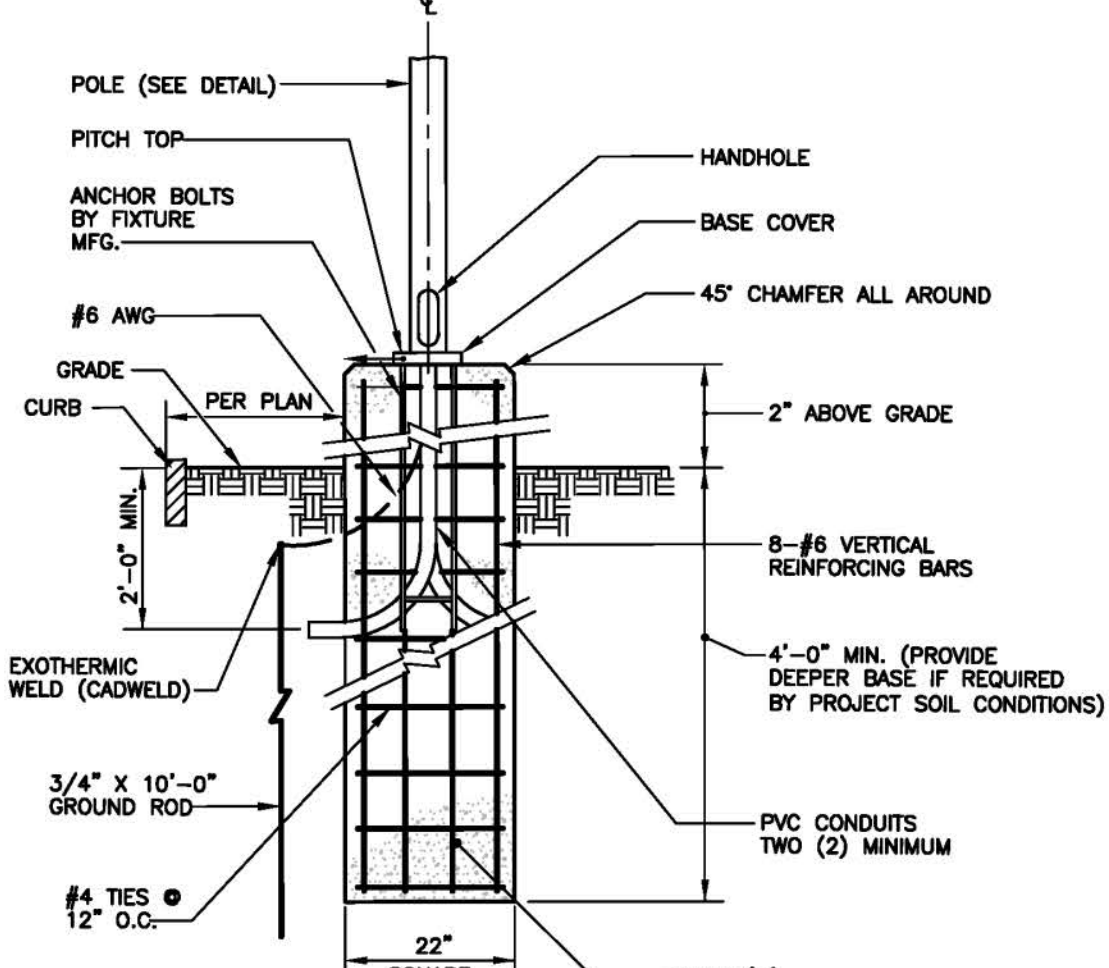
VALVE BOX (OR APPR. EQUAL) DETAIL
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER
 ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE
 N.T.S. (IF AND WHERE DIRECTED)



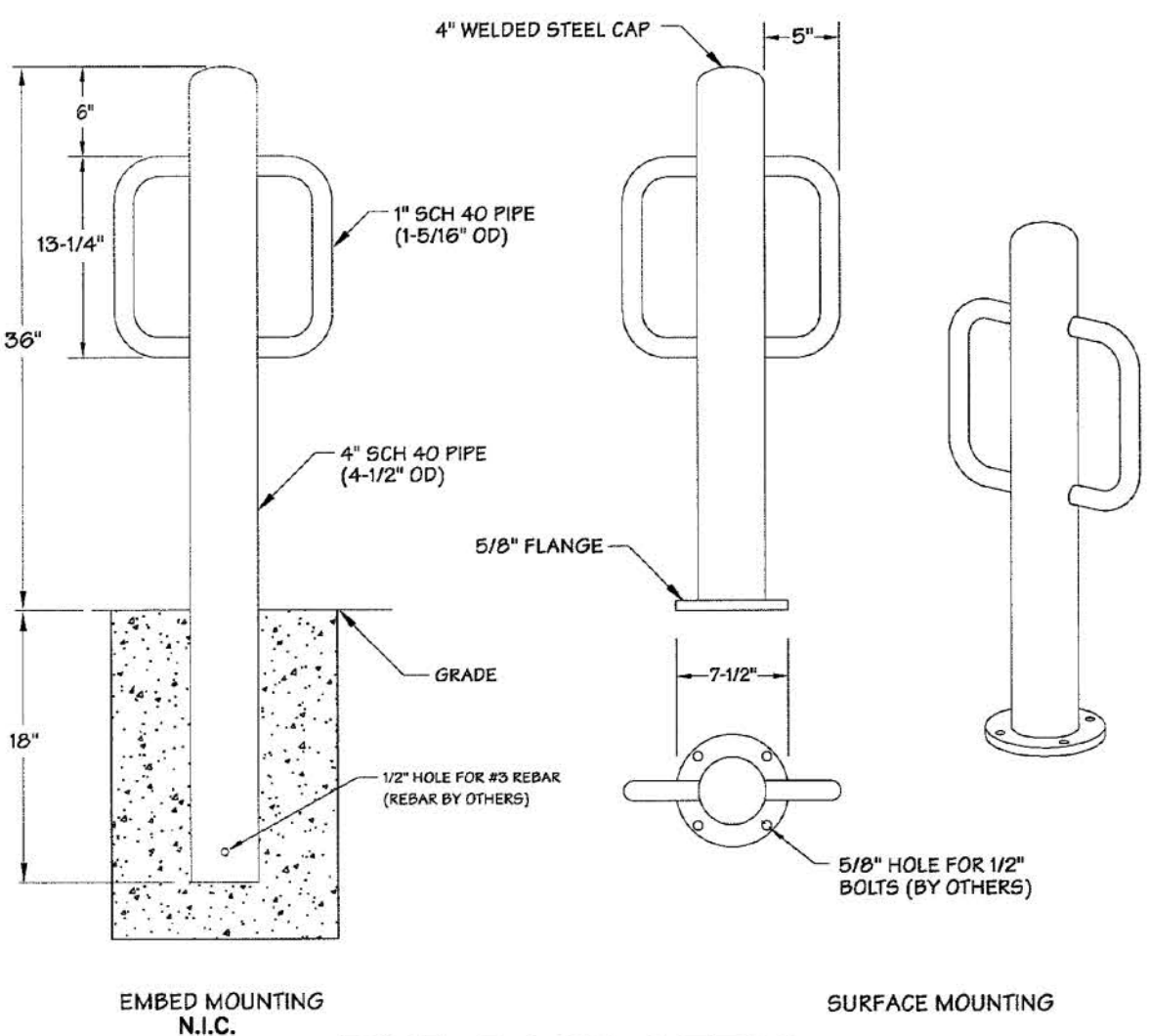
HANDHOLE/SPLICE BOX DETAIL
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER N.T.S. (OR APPR. EQUAL)



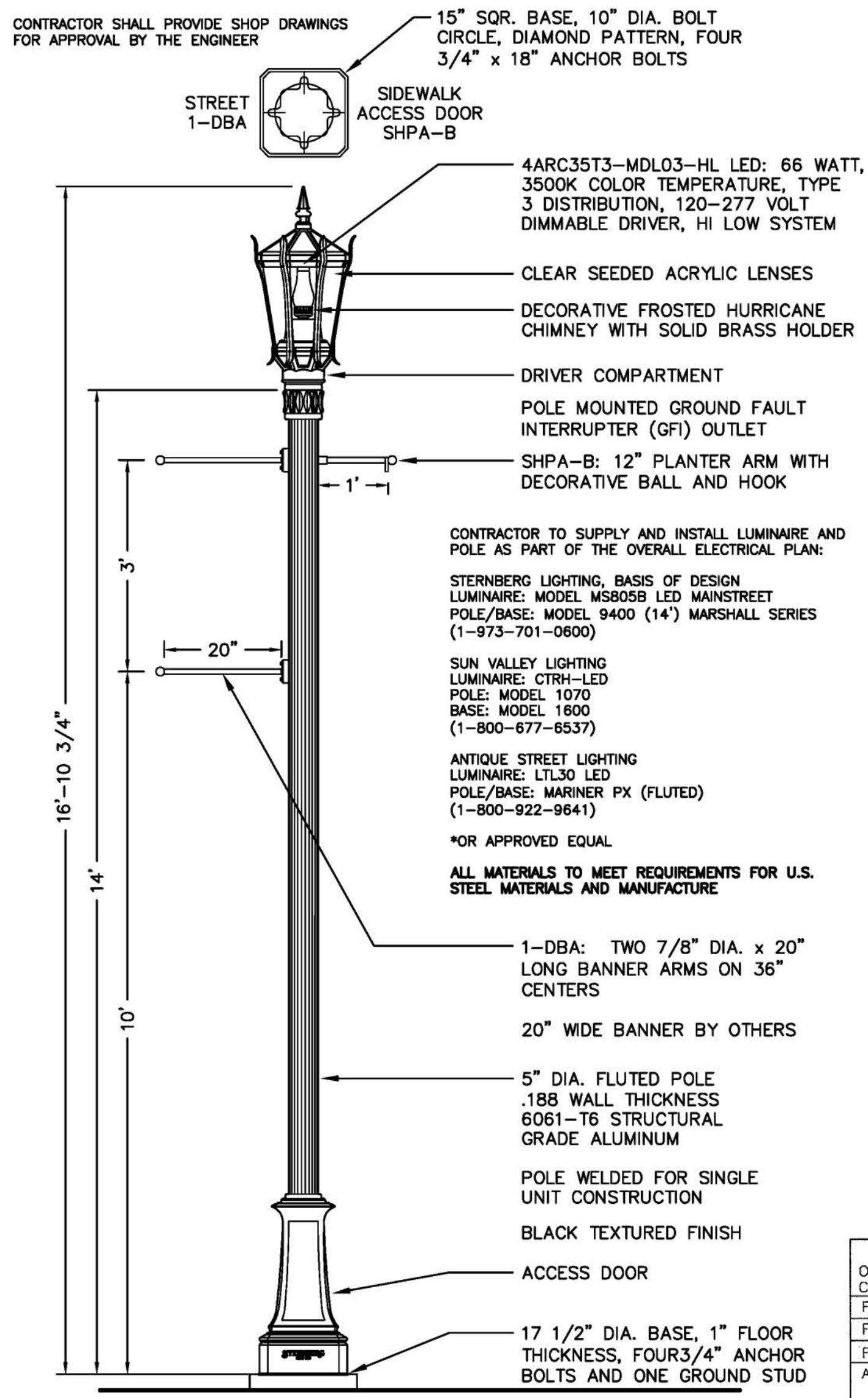
STREETSCAPE ENLARGEMENT DETAIL
 SCALE: 1" = 5'



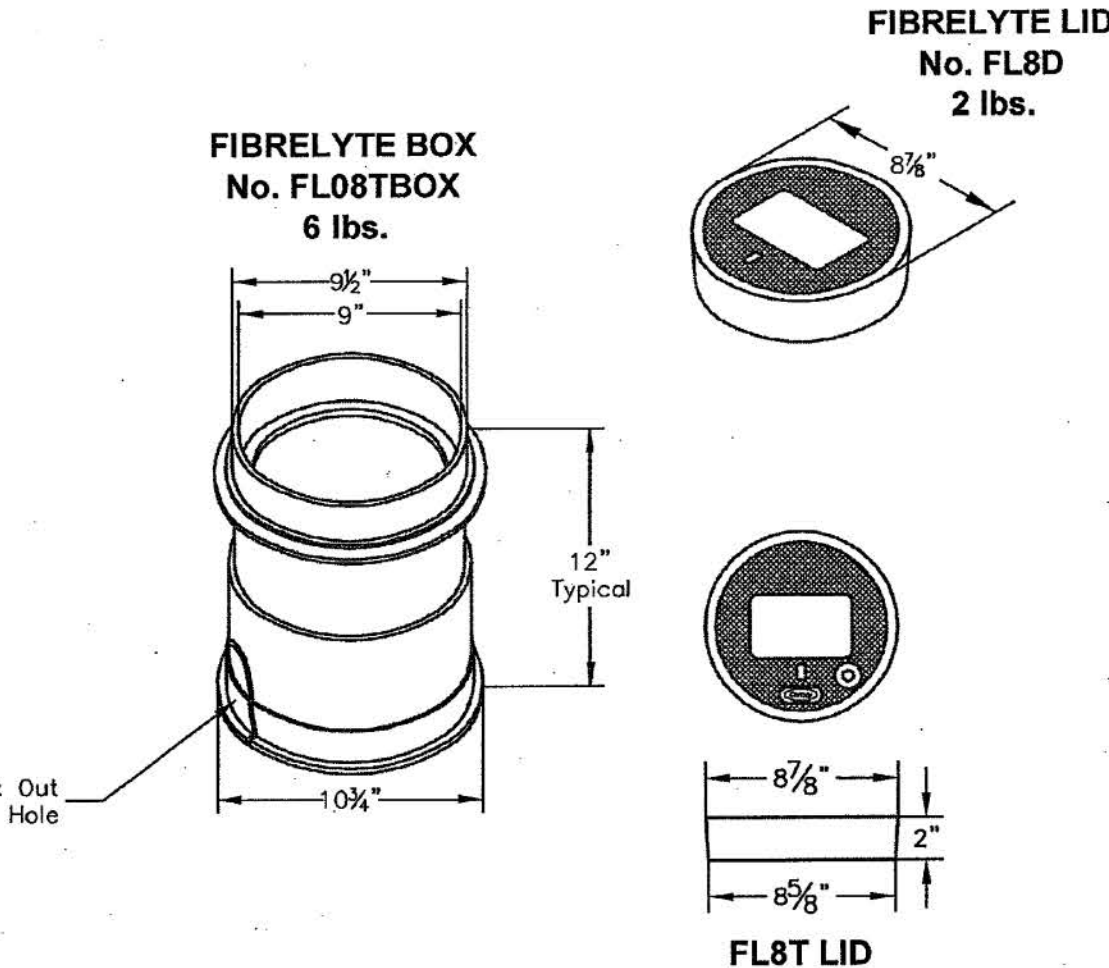
POLE BASE DETAIL
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER N.T.S.



BIKE RACK DETAIL
 BIKE BOLLARD, MODEL BR-2, SURFACE MOUNT AS MFG BY FAIRWEATHER SITE FURNISHINGS, PORT ORCHARD, WASHINGTON OR APPR. EQUAL N.T.S.



LUMINAIRE & POLE DETAIL
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER
 CONTRACTOR TO SUPPLY AND INSTALL LUMINAIRE AND POLE AS PART OF THE OVERALL ELECTRICAL PLAN:
 STERNBERG LIGHTING, BASIS OF DESIGN LUMINAIRE: MODEL MS805B LED MAINSTREET POLE/BASE: MODEL 9400 (14") MARSHALL SERIES (1-973-701-0600)
 SUN VALLEY LIGHTING LUMINAIRE: CTRH-LED POLE: MODEL 1070 BASE: MODEL 1800 (1-800-677-6537)
 ANTIQUE STREET LIGHTING LUMINAIRE: LTL30 LED POLE/BASE: MARINER PX (FLUTED) (1-800-922-9641)
 *OR APPROVED EQUAL
 ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE
 1-DBA: TWO 7/8" DIA. x 20" LONG BANNER ARMS ON 36" CENTERS
 20" WIDE BANNER BY OTHERS
 5" DIA. FLUTED POLE .188 WALL THICKNESS 6061-T6 STRUCTURAL GRADE ALUMINUM
 POLE WELDED FOR SINGLE UNIT CONSTRUCTION
 BLACK TEXTURED FINISH
 ACCESS DOOR
 17 1/2" DIA. BASE, 1" FLOOR THICKNESS, FOUR 3/4" ANCHOR BOLTS AND ONE GROUND STUD

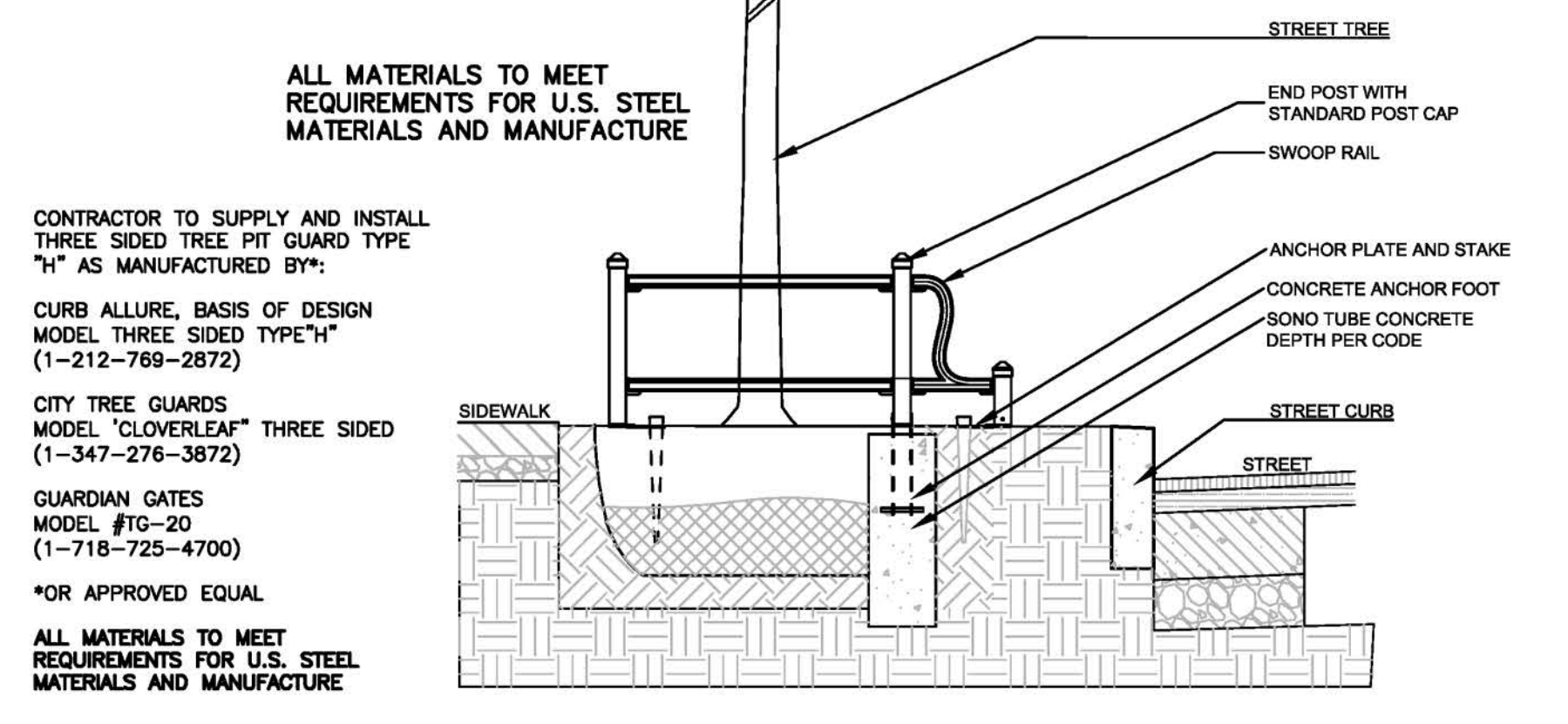


Ordering Code	Item	Approx. Shipping Weight	Description
FLO8TBOX	BOX	6	FLO8T Box (9" dia. I.D. x 12" high) - 64 per pallet
FLO8D	LID	2	Fibrelyte Lid
FLO8T	LID	2	Fibrelyte lid, bolt down (bolt included)
ADS	EXTENSION		ADS plastic extension 6", 8", 12", 18" & 10' Standard lengths. Others on request. No adapter required.

VALVE BOX (OR APPR. EQUAL) DETAIL
 N.T.S.



STREET SIGN DETAIL
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER N.T.S.



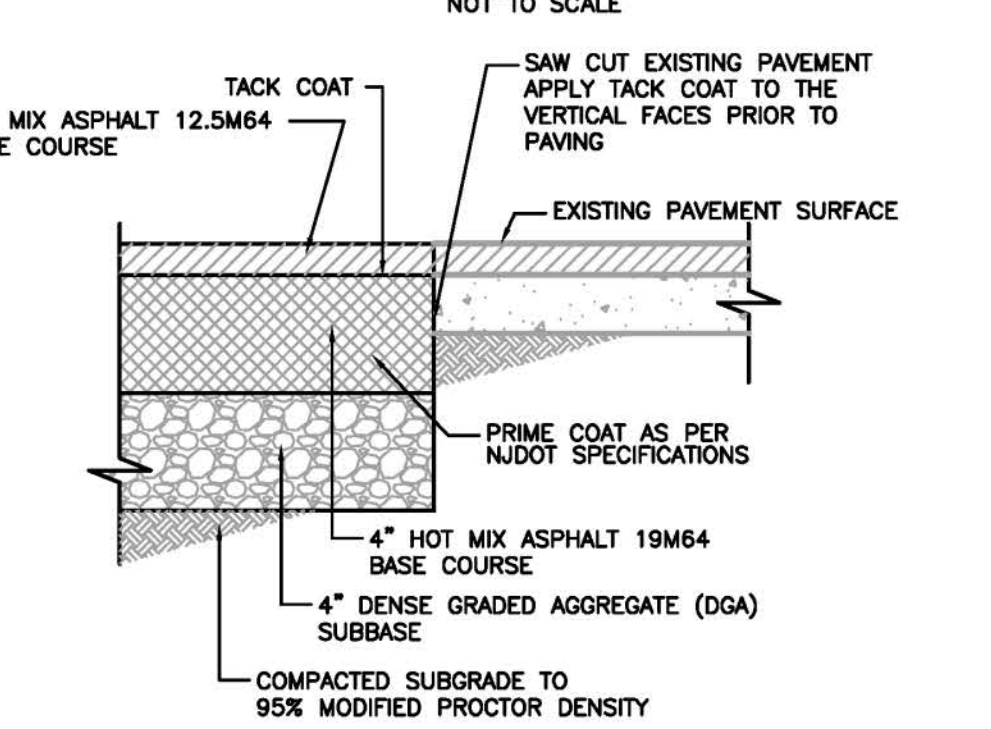
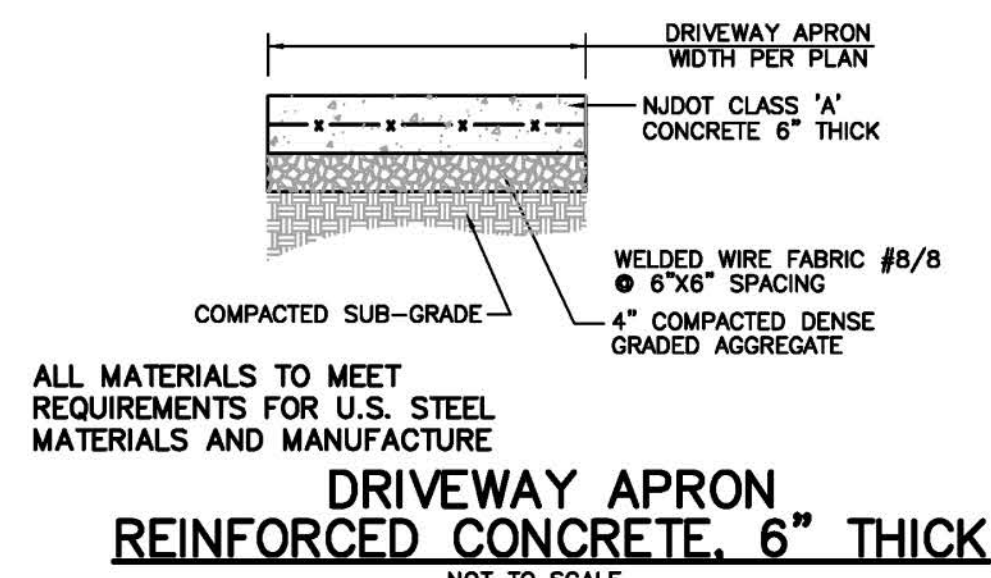
TREE PIT GUARD TYPE 'H'
 SCALE: NTS
TYPE 'H' THREE SIDED DESIGN
 CONTRACTOR SHALL PROVIDE SHOP DRAWINGS FOR APPROVAL BY THE ENGINEER

NO.	REVISIONS	DATE	BY
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 PROJ.: 2016020
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 REVISIONS

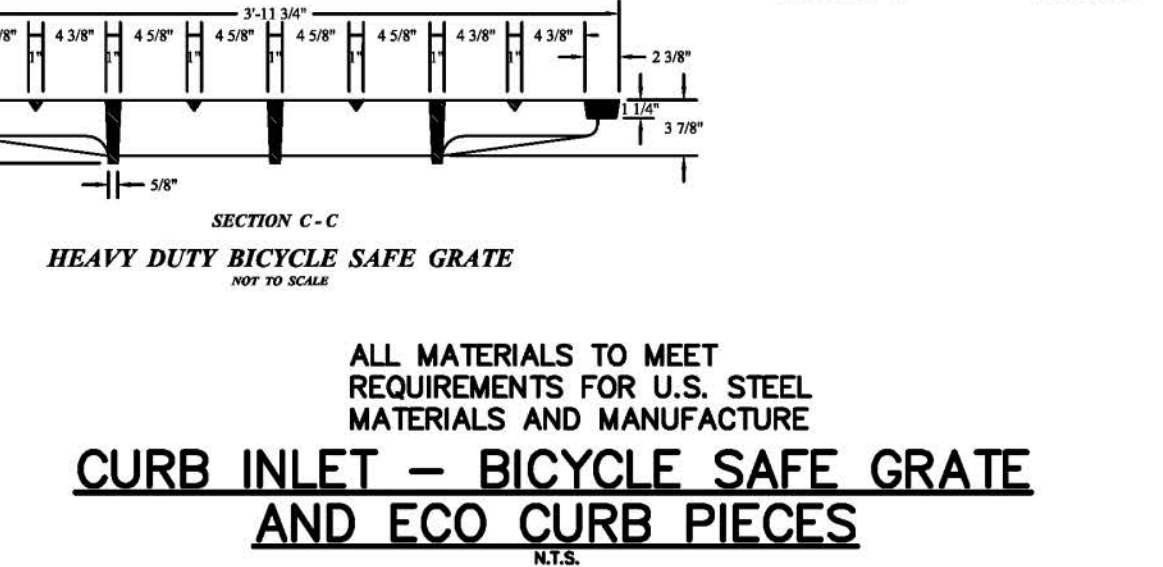
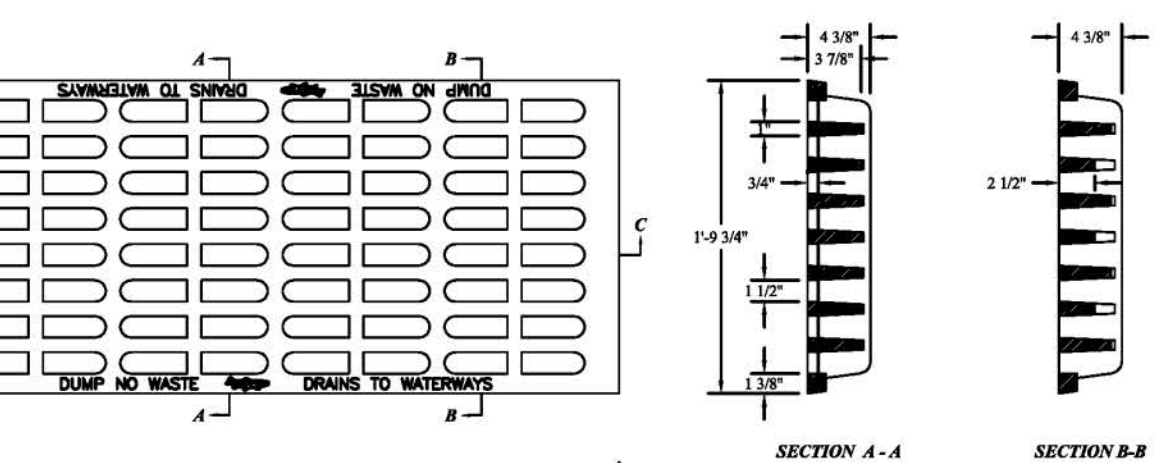
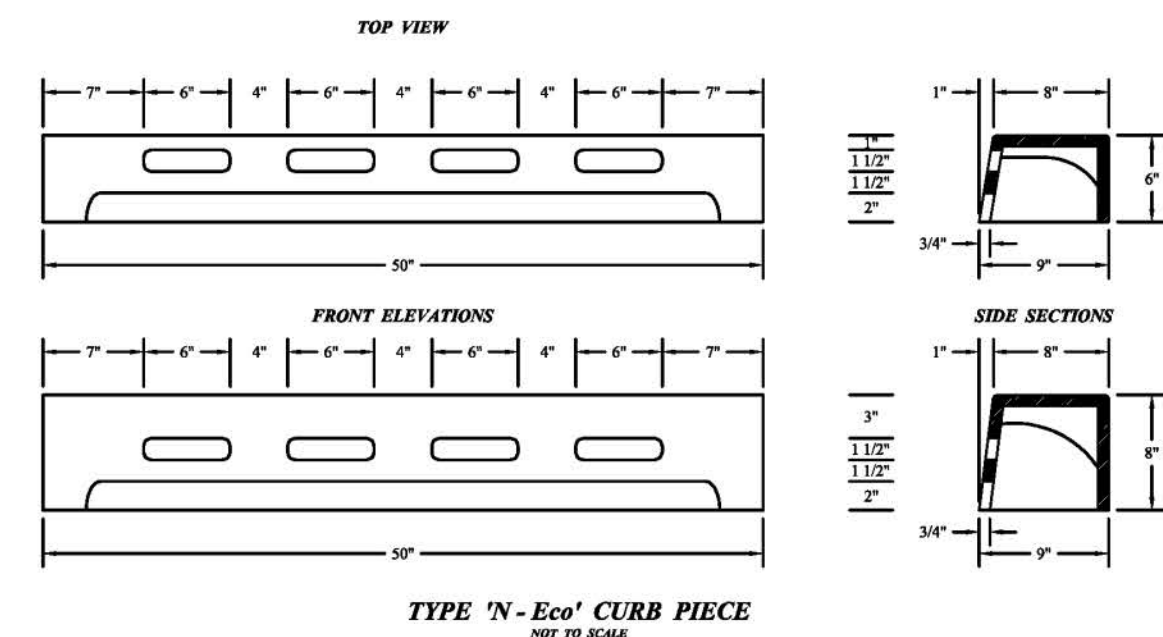
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 200 Gibraltar Drive, Suite 204, Morris Plains, NJ 07960 973-377-8600
 CONSULTING ENGINEERS
 LAND SURVEYORS
 LANDSCAPE ARCHITECTS
 PLANNERS
 ANDREW CANGIANO, Professional Engineer, Lic. 43117

MAIN STREET SIDEWALK & STREETSCAPE
 BLOOMFIELD AVENUE TO MINE STREET
 CONSTRUCTION DETAILS
 SHEET No. 25 OF 31
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)



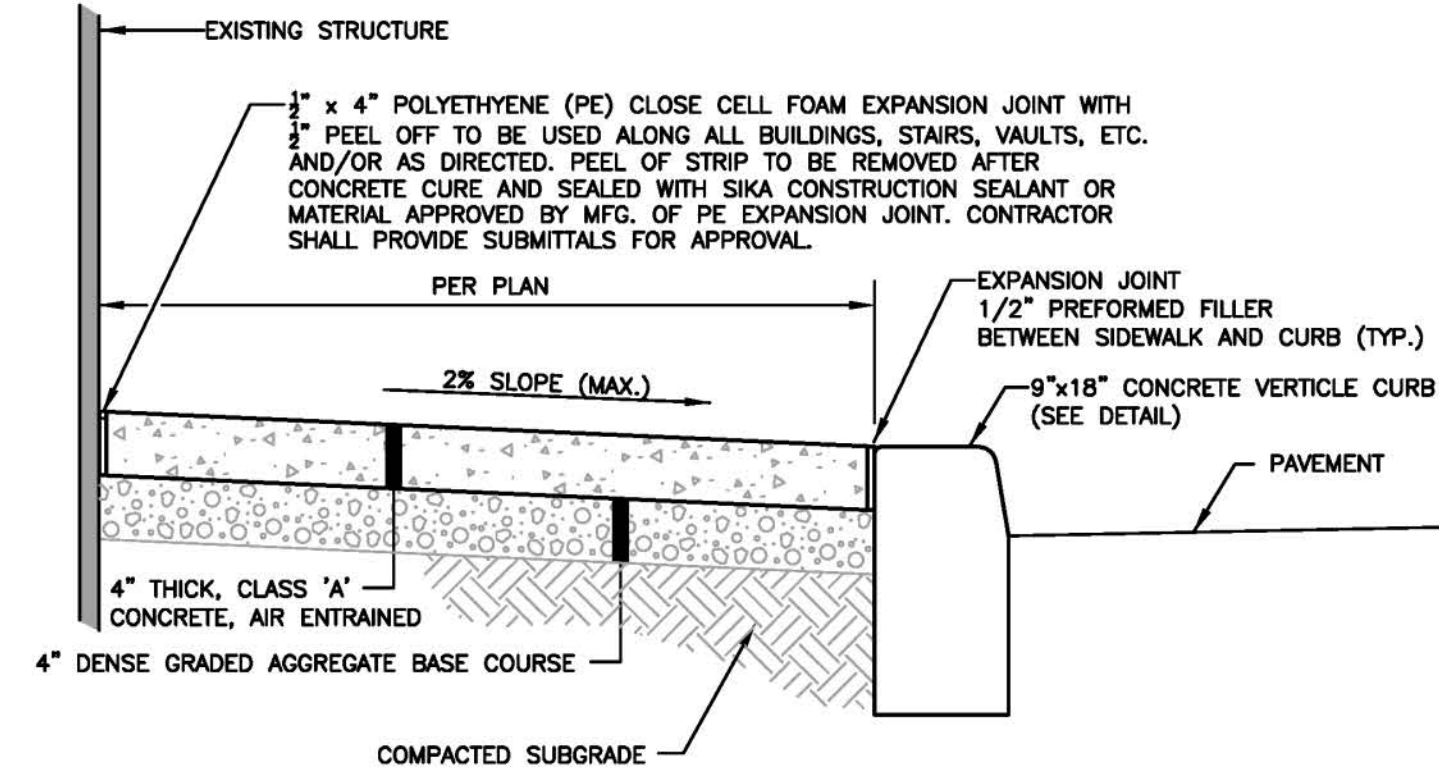
NOTES:
 1. CONTRACTOR SHALL RESET ALL EXISTING UTILITY ACCESS FRAME AND COVERS AS NECESSARY TO MATCH THE PROPOSED GRADE.
 2. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR RE-SAW CUTTING EXISTING PAVEMENT IF DAMAGED. SAW CUT SHALL BE LOCATED AND MADE AT THE DISCRETION OF THE BOROUGH ENGINEER.
 3. ALL EXISTING ROAD BED CONCRETE TO BE REMOVED AND DISPOSED OF WITHIN EXCAVATION FOR CURB AND ROAD REPAIR.

FULL DEPTH HOT MIX ASPHALT PAVEMENT DETAIL
 N.T.S.

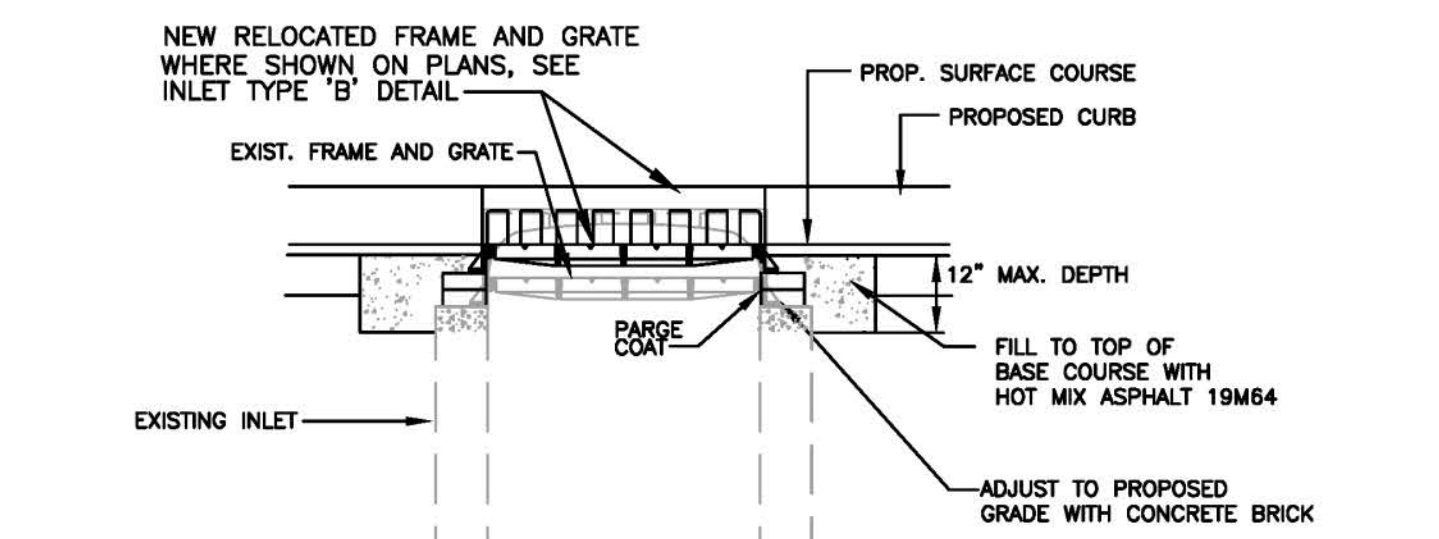


CURB INLET - BICYCLE SAFE GRATE AND ECO CURB PIECES
 N.T.S.

ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE

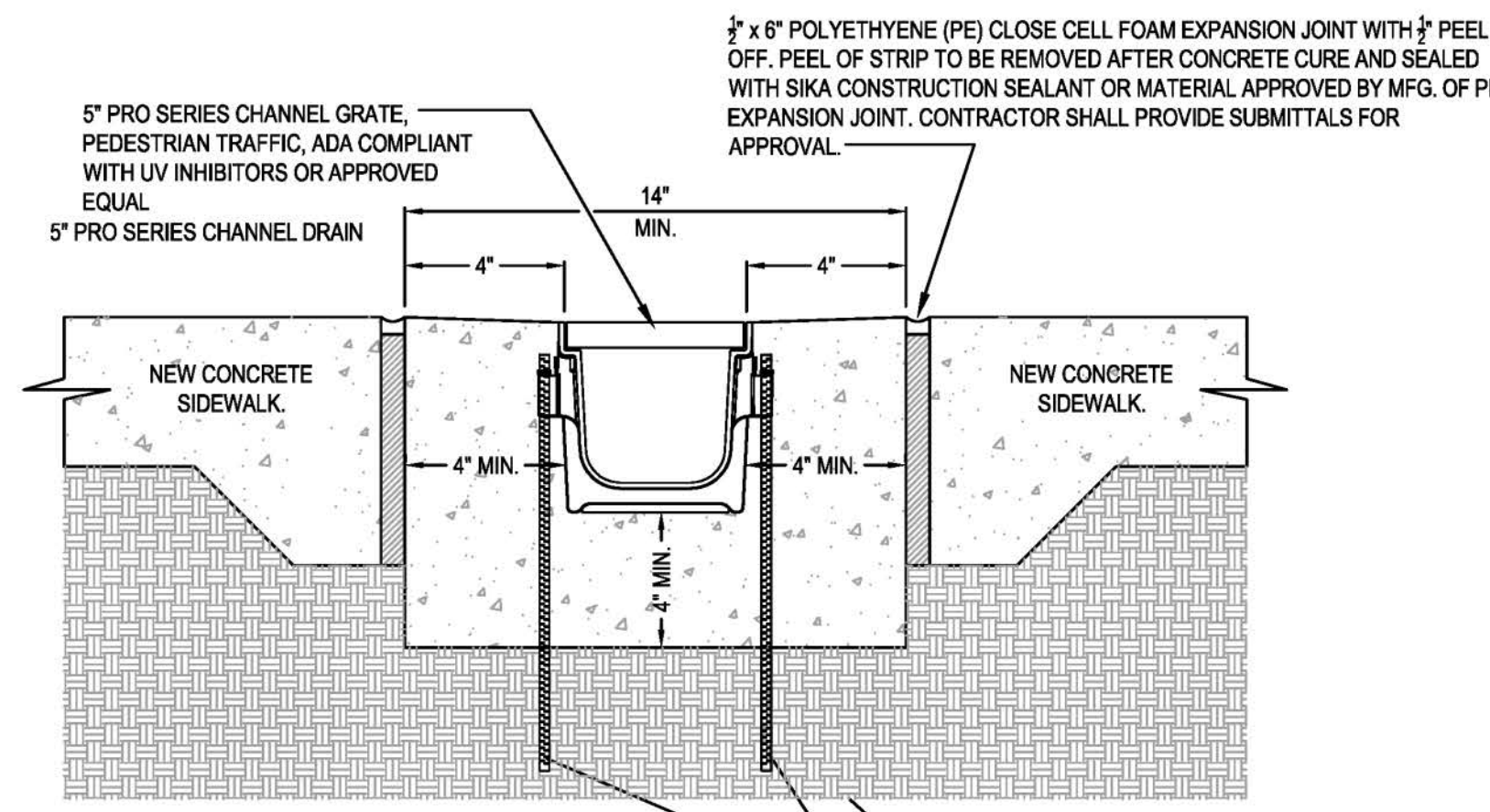


CONCRETE SIDEWALK ABUTTING GRANITE BLOCK CURB/STRUCTURE
 N.T.S.



RESET EXISTING INLET FRAME AND GRATE
 N.T.S.

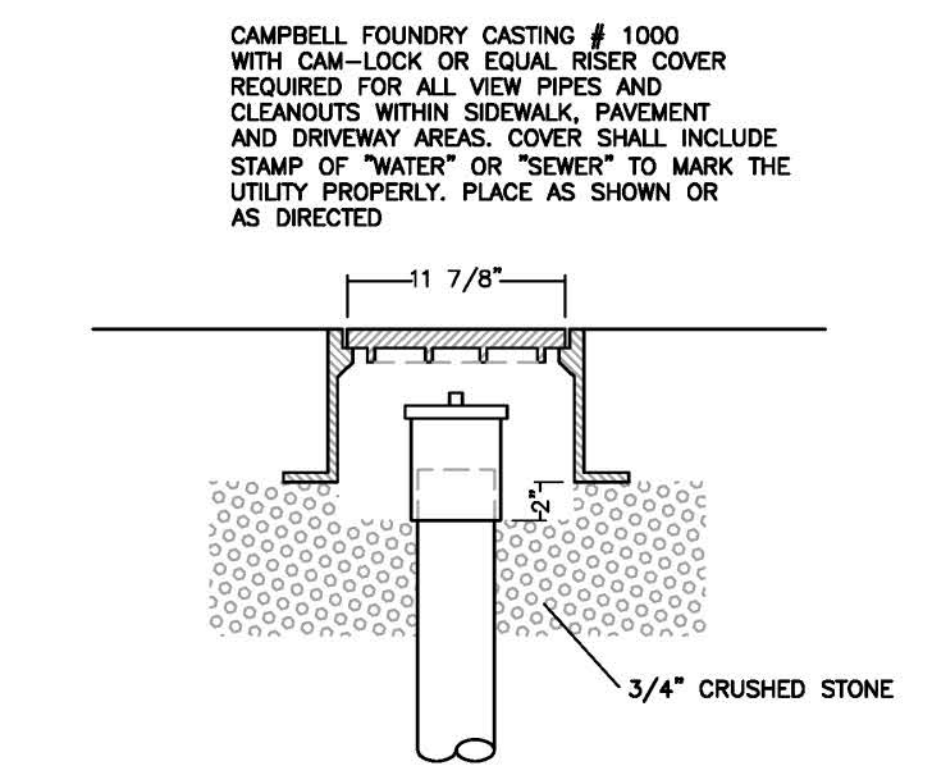
ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE



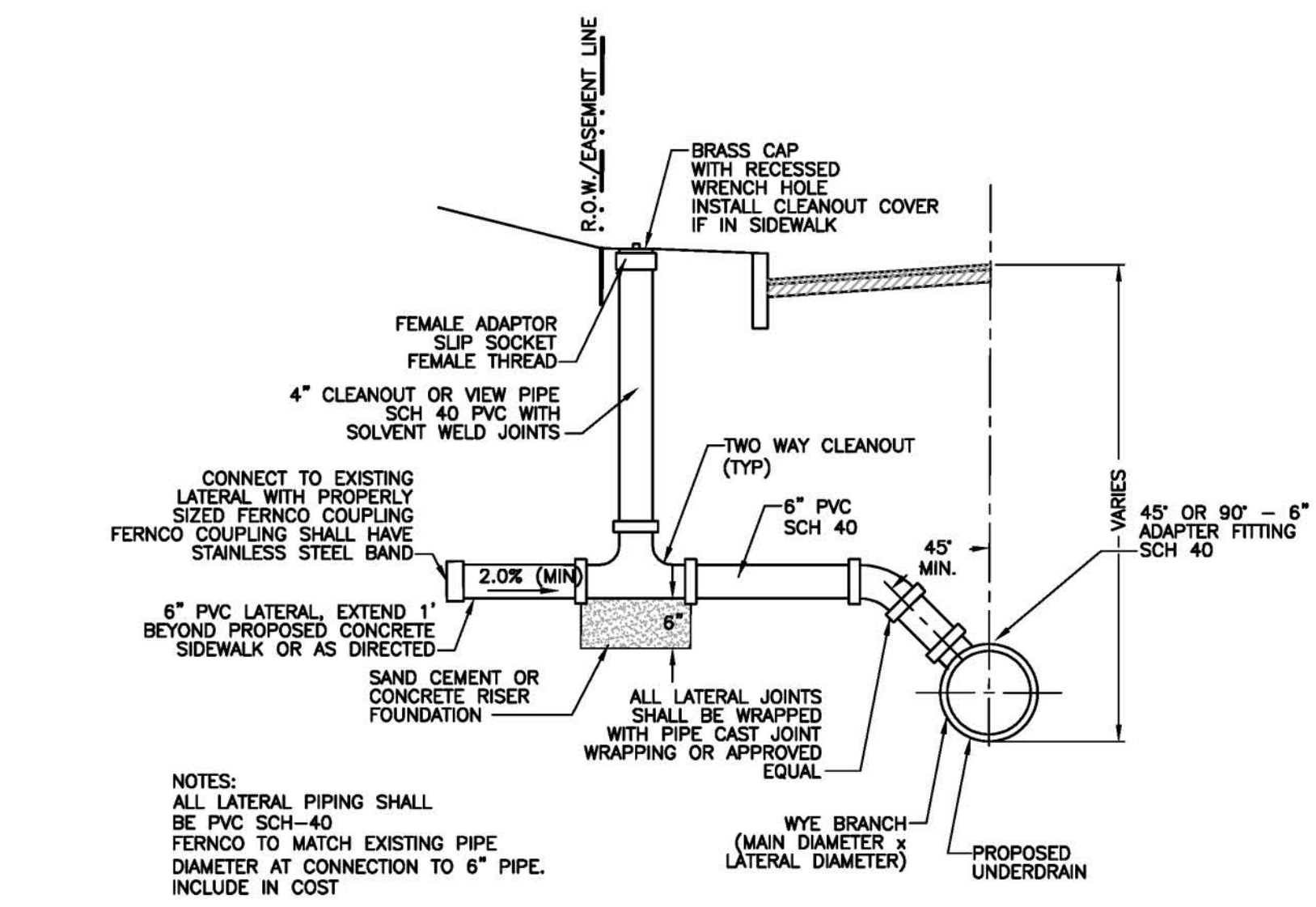
ALL MATERIALS TO MEET REQUIREMENTS FOR U.S. STEEL MATERIALS AND MANUFACTURE

NOTES:
 1. CHANNELS TO BE INSTALLED WITH BLANK GRATE. GRATE TO BE PROTECTED FROM CONCRETE POUR (COVER HOLES WITH TAPE).
 2. INSTALLATION TO BE COMPLETED IN ACCORDANCE WITH MANUFACTURER'S SPECIFICATIONS.
 3. DO NOT SCALE DRAWING.
 4. SHOP DRAWINGS TO BE SUBMITTED FOR APPROVAL PRIOR TO INSTALLATION

TRENCH DRAIN IN CONCRETE SIDEWALK
 (OR APPROVED EQUAL - NTS - IF AND WHERE DIRECTED)

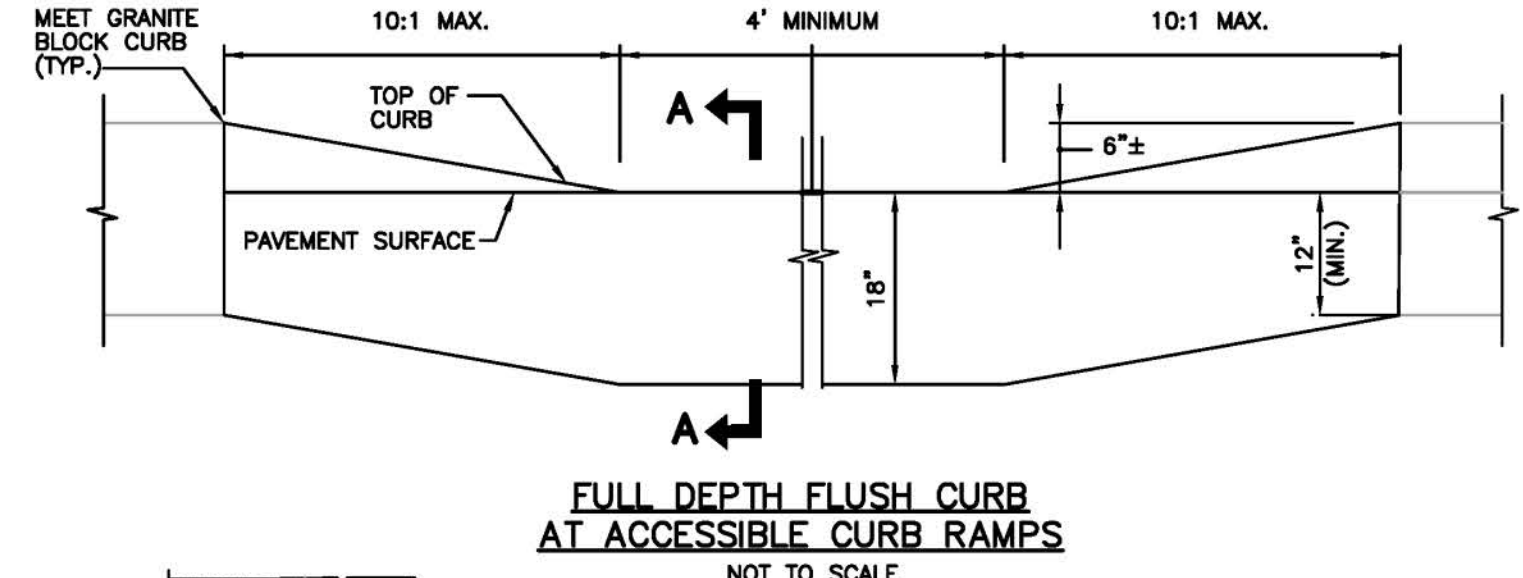


STORM SEWER CLEANOUT AND WATER SERVICE COVER DETAIL
 NOT TO SCALE

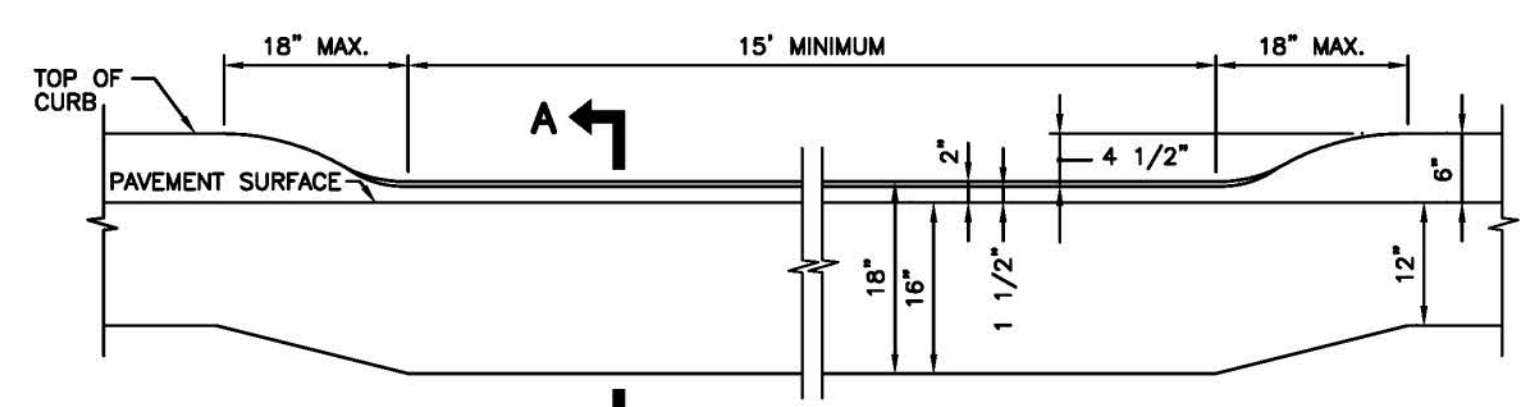
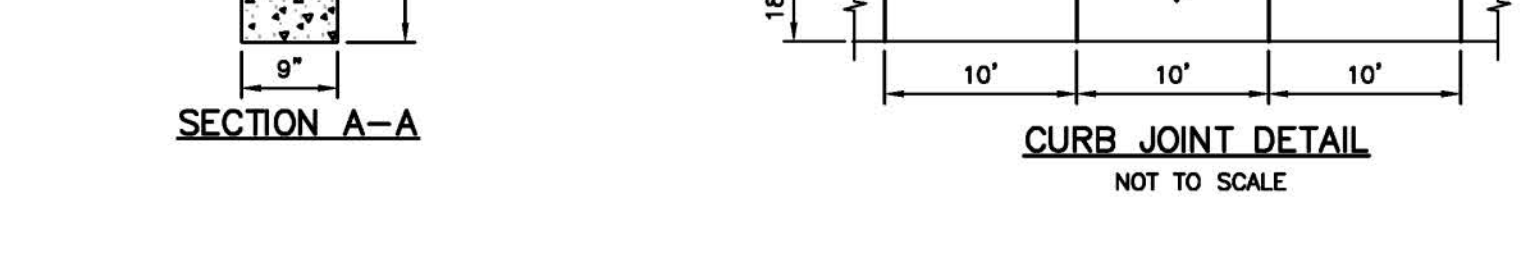


STORM SEWER LATERAL CONNECTION TO PROPOSED UNDERDRAIN
 NOT TO SCALE

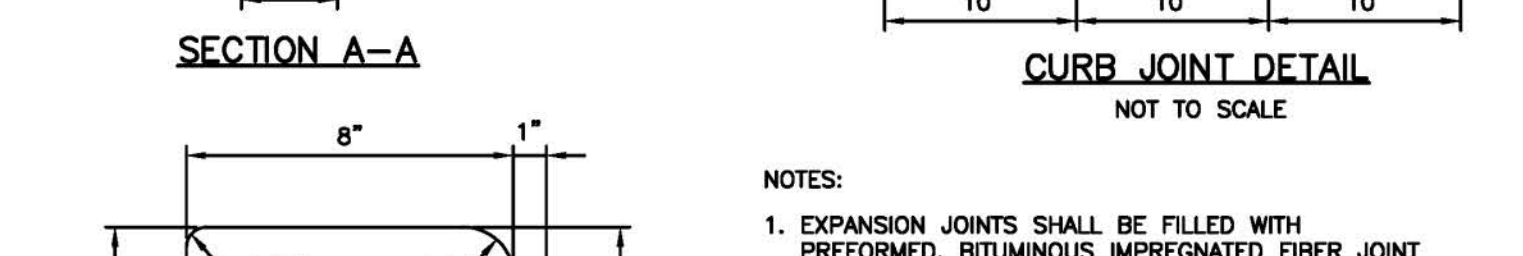
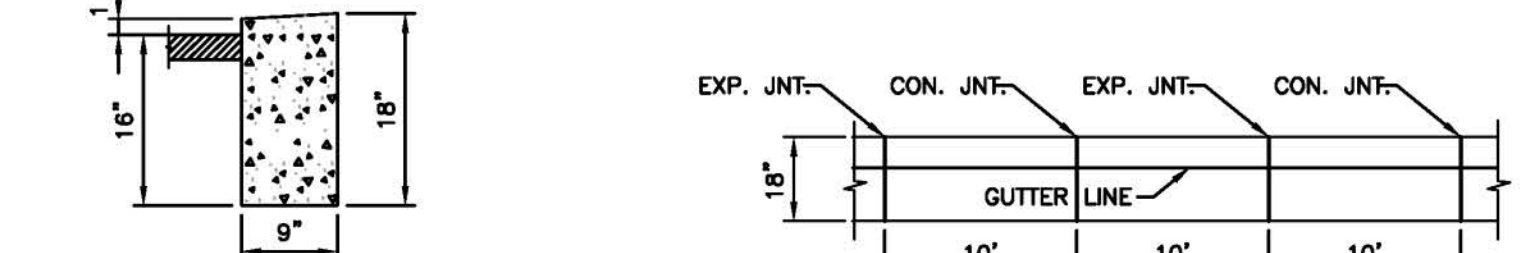
NOTES:
 ALL LATERAL PIPING SHALL BE PVC SCH-40 FERNCO TO MATCH EXISTING PIPE DIAMETER AT CONNECTION TO 6" PIPE. INCLUDE IN COST



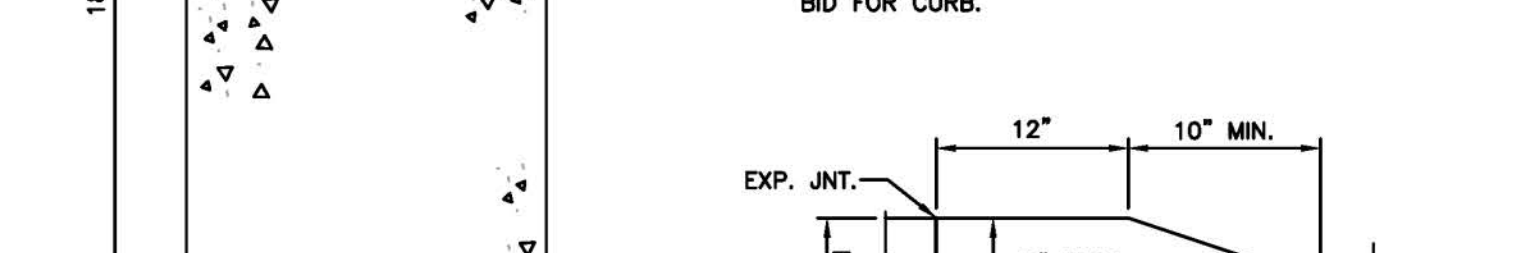
FULL DEPTH FLUSH CURB AT ACCESSIBLE CURB RAMPS
 NOT TO SCALE



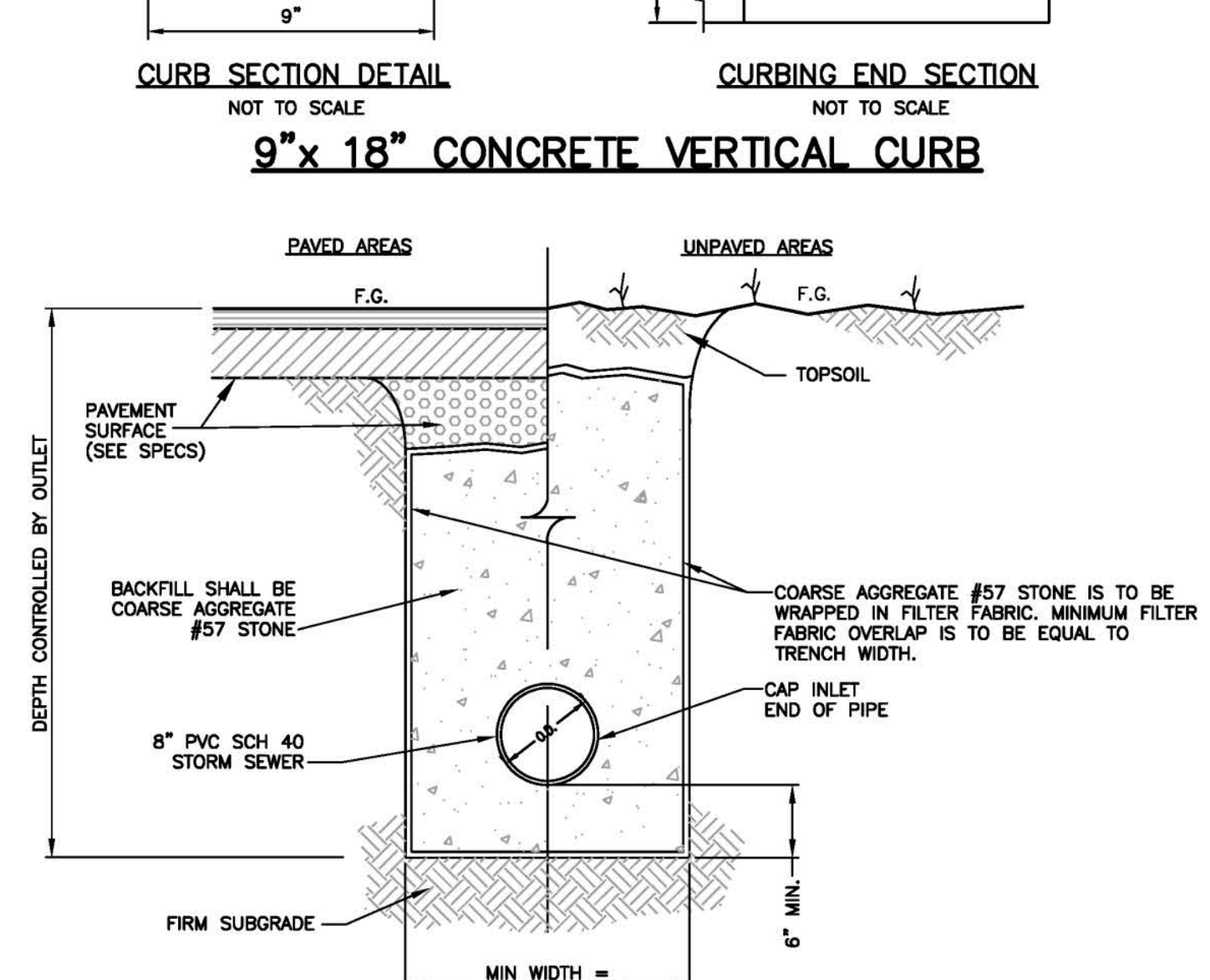
FULL DEPTH DEPRESSED CURB AT DRIVEWAYS
 NOT TO SCALE



NOTES:
 1. EXPANSION JOINTS SHALL BE FILLED WITH PREFORMED, BITUMINOUS IMPREGNATED FIBER JOINT FILLER, RECESSED 1/4" FROM FACE AND TOP OF CURB.
 2. EXPANSION JOINTS SHALL BE INSTALLED BETWEEN CURB AND INLET HEADS.
 3. EXPANSION JOINTS SHALL ALTERNATE WITH CONSTRUCTION JOINTS EVERY 10' (SEE DETAIL ABOVE)
 4. ALL JOINTS SHALL EXTEND FULL 18" DEPTH OF CURB.
 5. EXPANSION JOINTS THROUGH AND ADJACENT TO THE CURB SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CURB.



9" x 18" CONCRETE VERTICAL CURB
 NOT TO SCALE



NOTES:
 1. PIPE JOINTS SHALL BE GLUED WITH FITTINGS PER MANUFACTURER
 2. TIE ALL DRAIN LINES DIRECTLY INTO 8" PVC USING 4" OR 6" DIAMETER PLASTIC PIPE AND THE NECESSARY PIPE FITTINGS AND CONNECTIONS.
 3. BEGINNING END OF PIPE EXPOSED TO STONE IS TO BE CAPPED WITH FILTER FABRIC.

UNDERDRAIN TYPE "F"
 NOT TO SCALE

6	NO REVISION THIS SHEET	6/13/18
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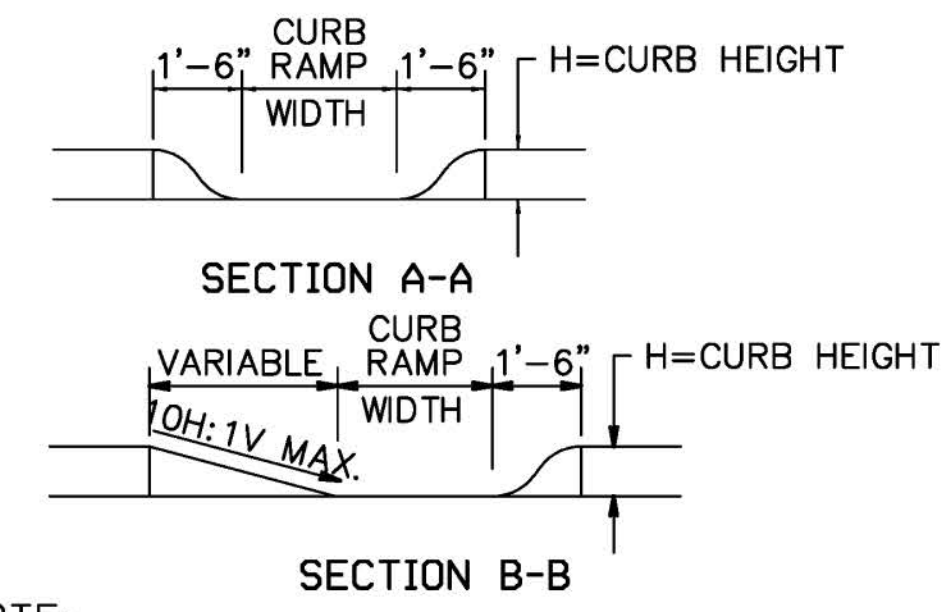
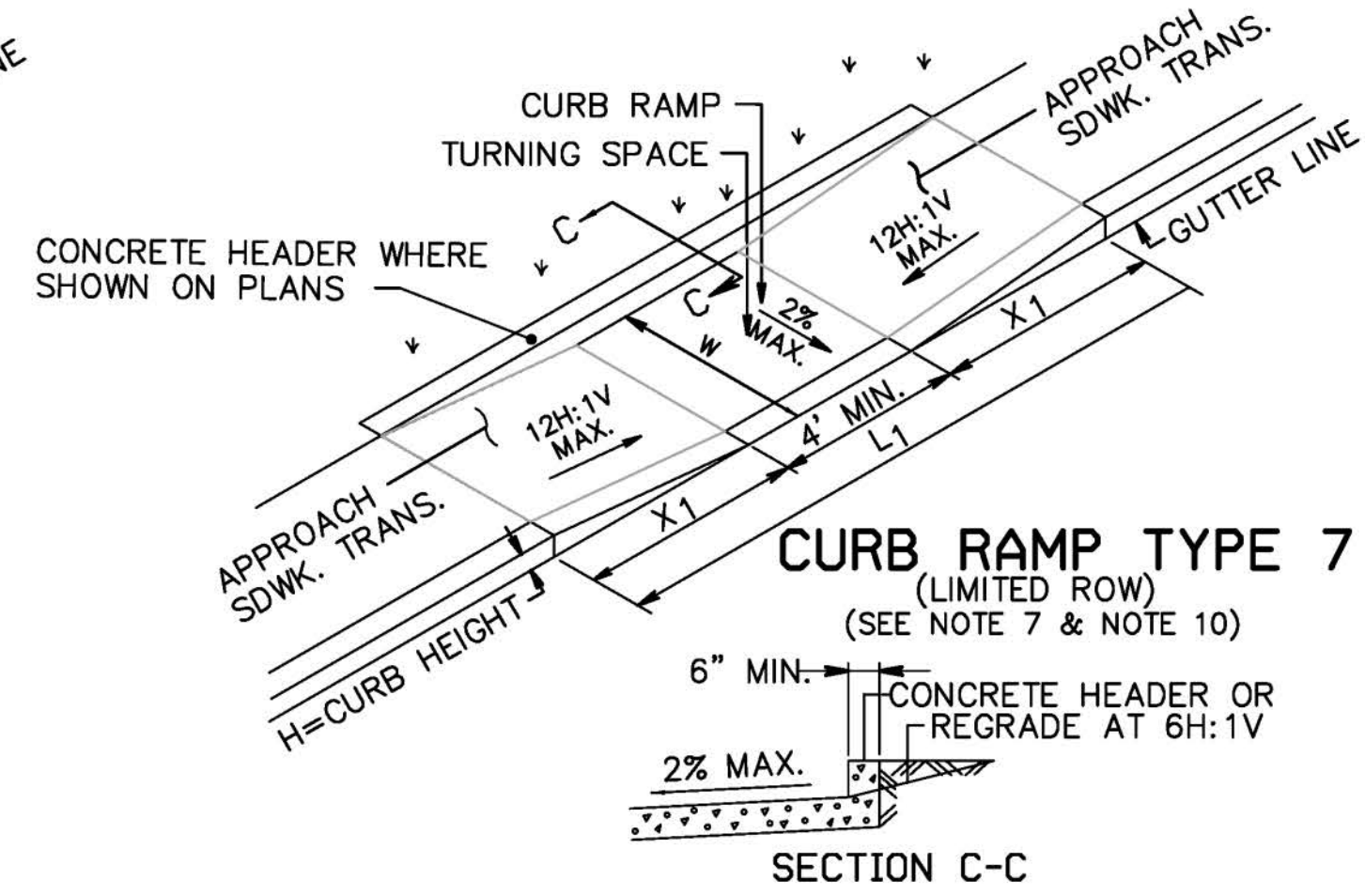
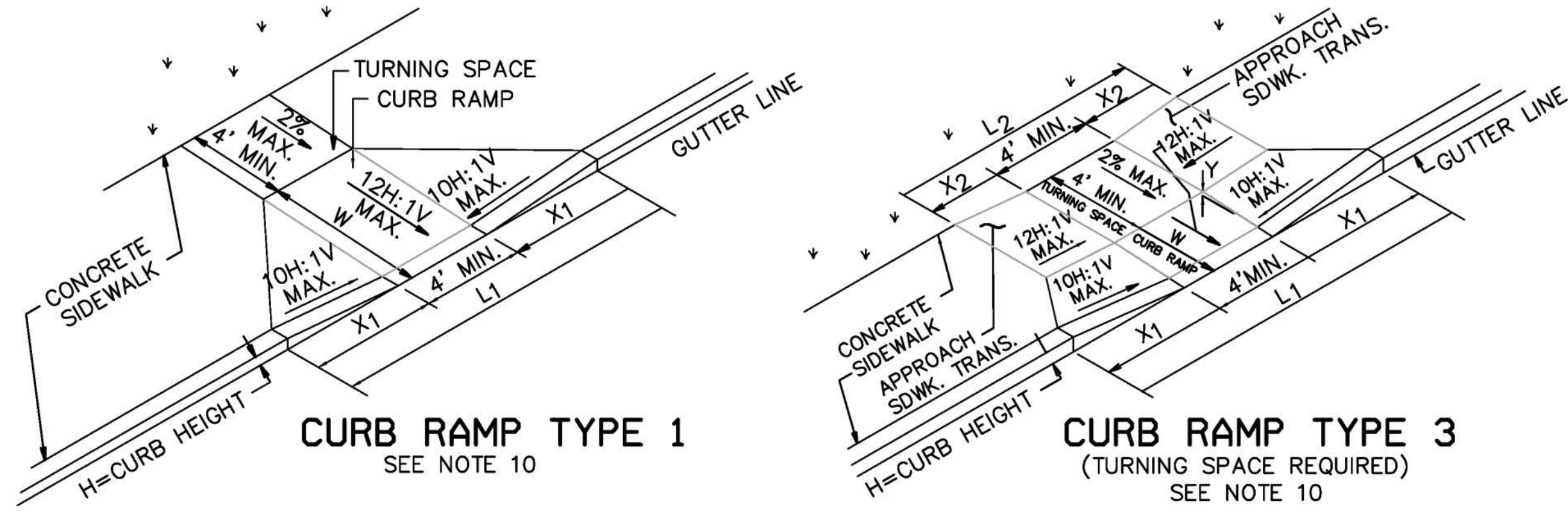
Andrew Cangiano
 ANDREW CANGIANO, Professional Engineer, Lic. 43111

MAIN STREET SIDEWALK & STREETSCAPE
 BLOOMFIELD AVENUE TO MINE STREET

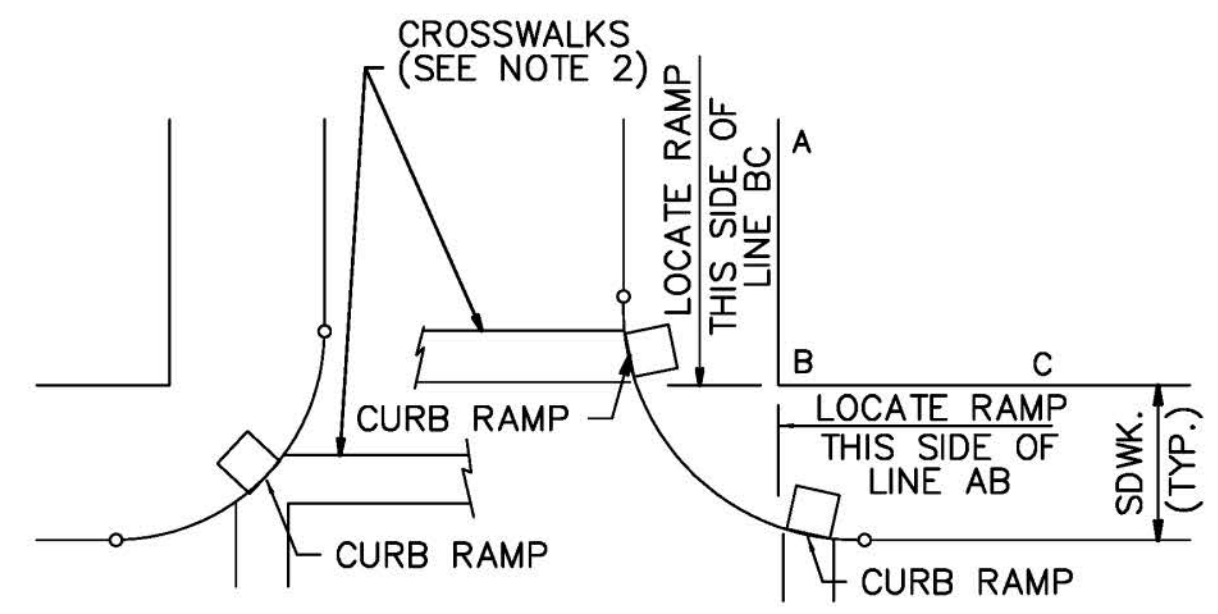
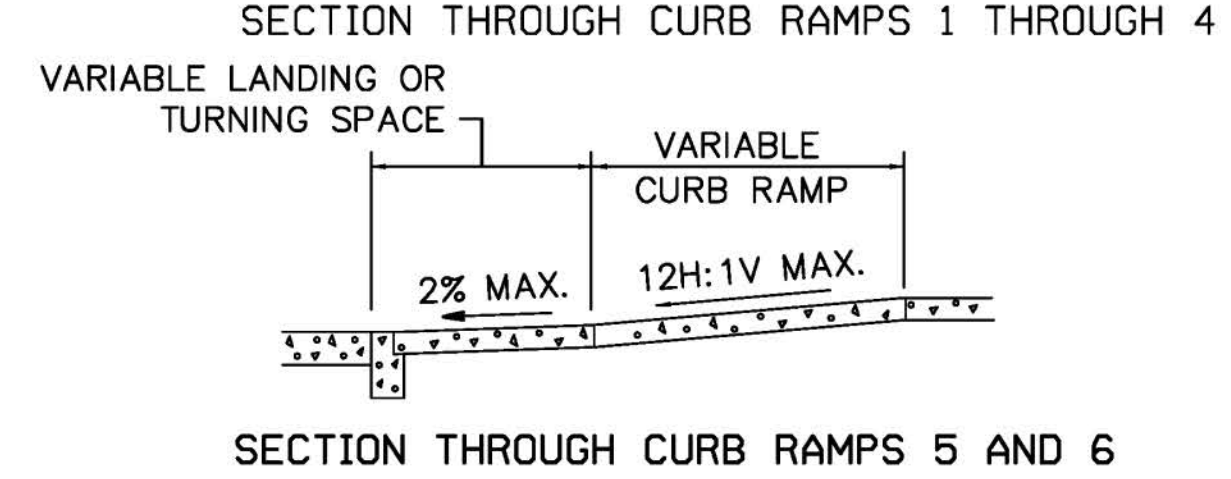
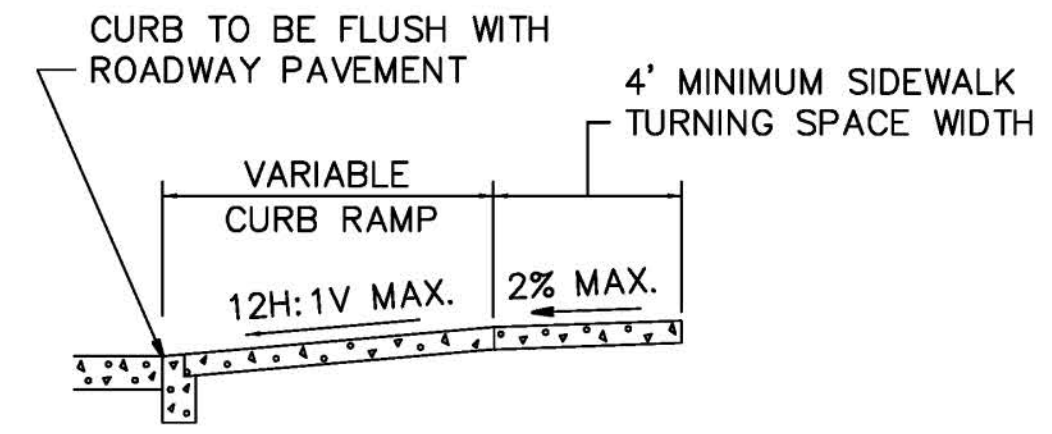
CONSTRUCTION DETAILS

BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(335)

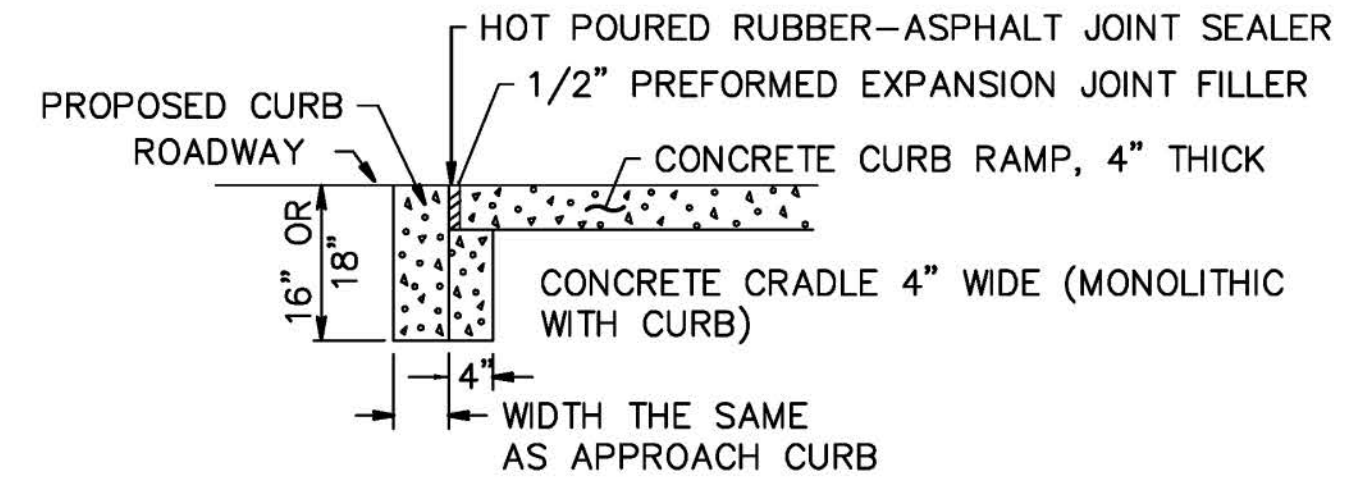
SHEET No. **26** OF **31**



NOTE:
CURB RAMP OPENING TO BE FLUSH WITH ROADWAY PAVEMENT (CURB RAMP TYPES 5 & 6).

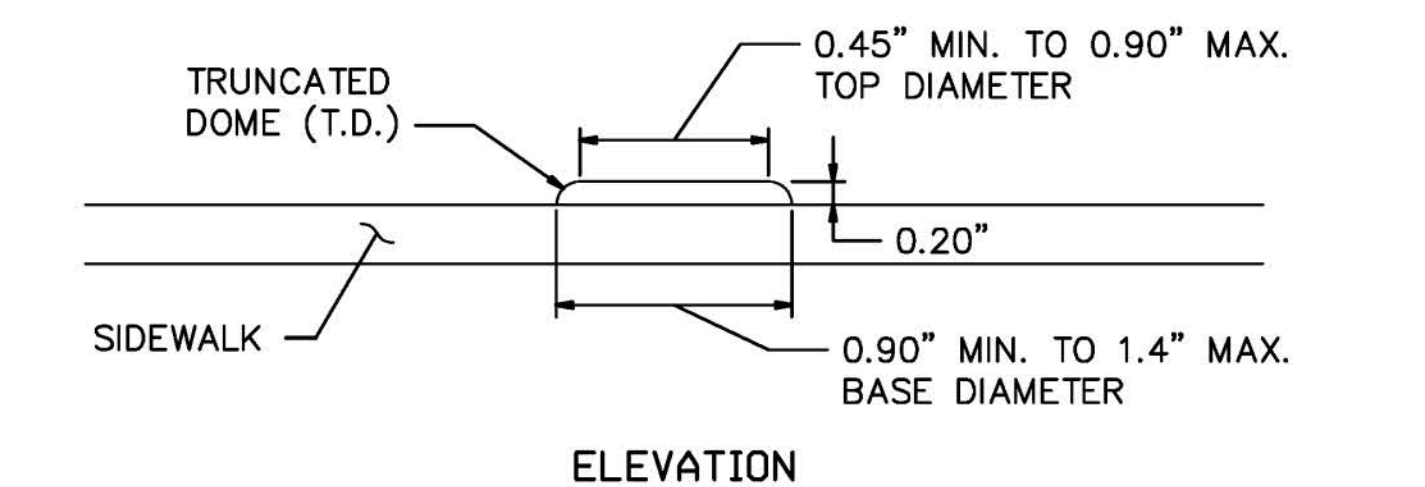
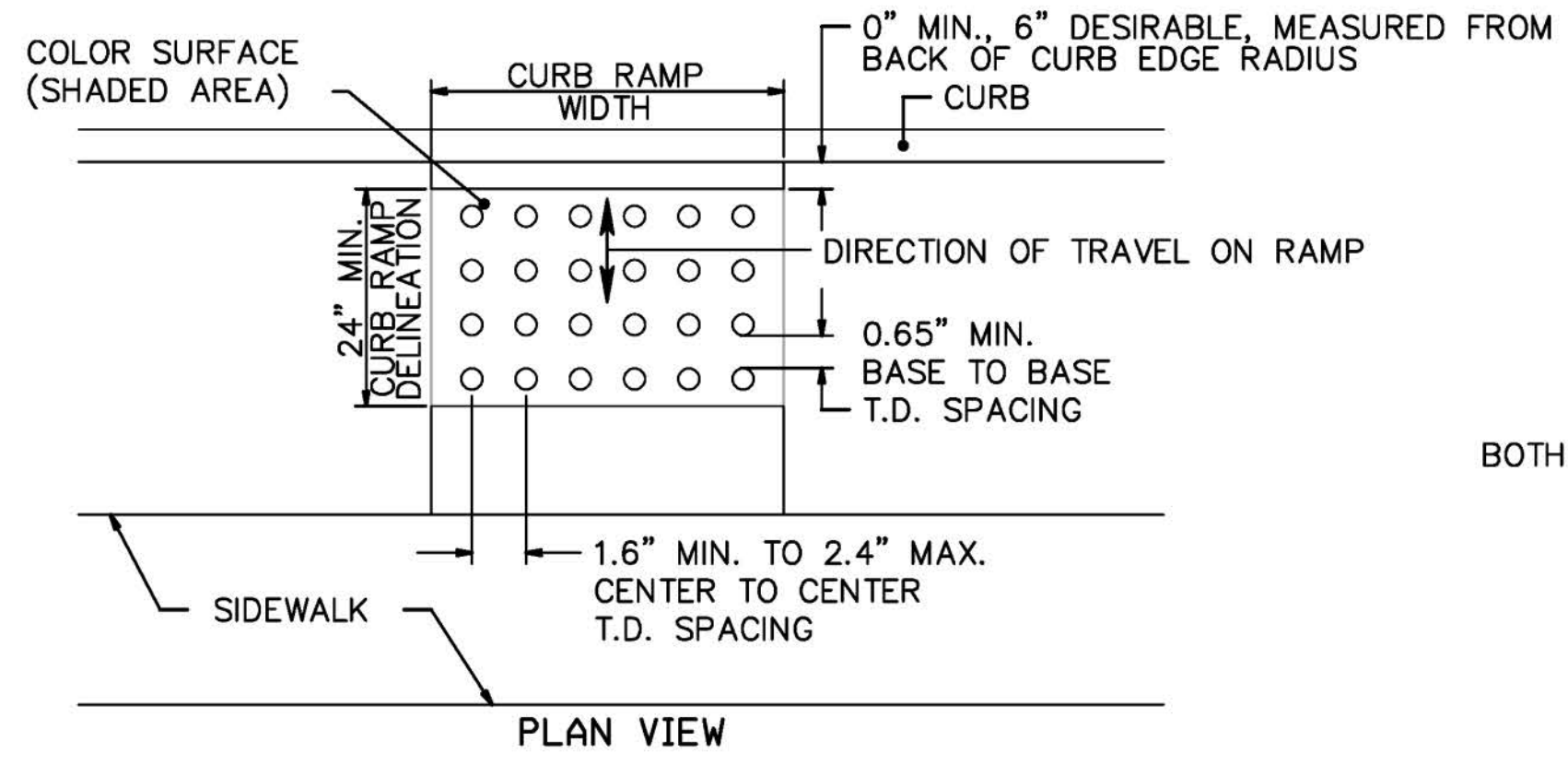


LOCATION OF CURB RAMP TYPES 1, 2, 3, 4, & 7 FOR CROSSING PARALLEL AND PERPENDICULAR TO HIGHWAY

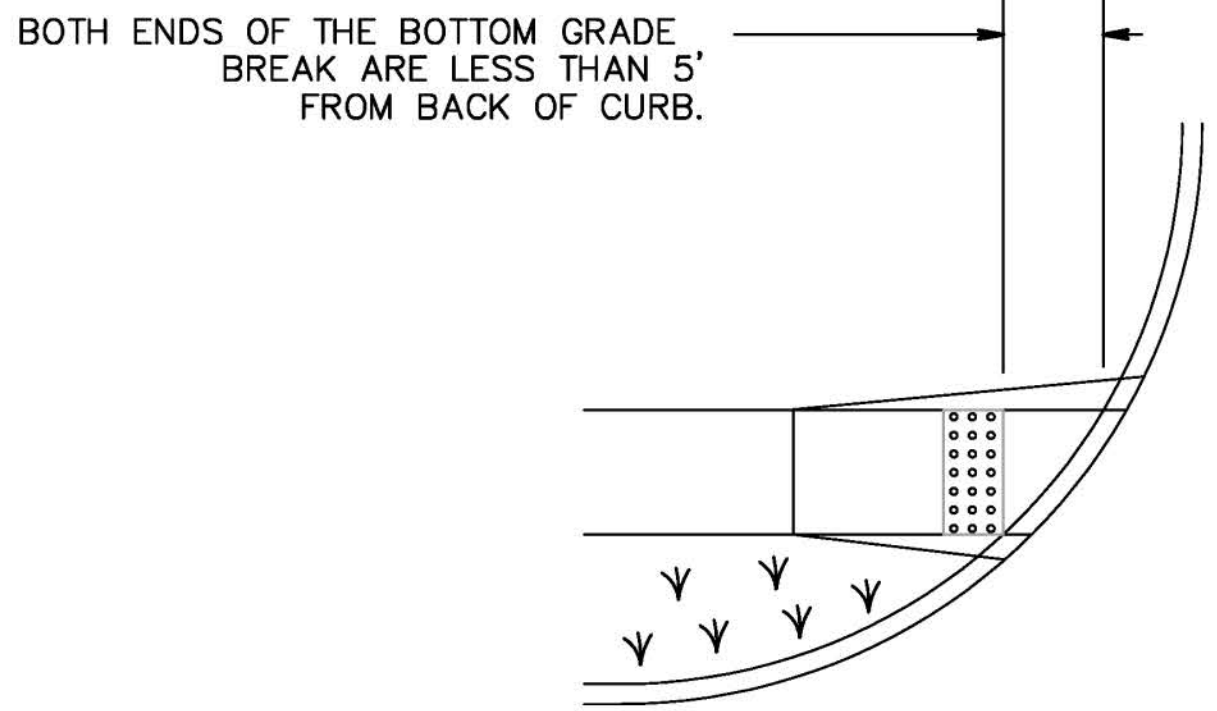


FLUSH CURB AND CRADLE

- CURB RAMP NOTES:**
- KEEP TURNING SPACE, APPROACH SIDEWALK TRANSITIONS, AND CURB RAMP CLEAR OF OBSTRUCTIONS THAT PROTRUDE ABOVE THE SURFACE.
 - FOR DIMENSIONS SEE CD-606-3 AND CD-606-4
 - CURB (DROPPED CURB) GUTTERLINE TO BE FLUSH WITH ROADWAY PAVEMENT THE ENTIRE WIDTH OF THE RAMP (4 FEET MIN.) AT ALL CURB RAMPS.
 - FOR CURB RAMP TYPES 5 AND 6, IF A GRASS BUFFER DOES NOT EXIST, SLOPE CURB TO EQUAL SLOPE OF ADJACENT CURB RAMP.
 - SIDEWALK AND CURB RAMP WITHIN AREA ENCLOSED BY HEAVY LINES INDICATES THE PAY LIMIT FOR CONCRETE SIDEWALK OF THE APPROPRIATE ADJACENT THICKNESS.
 - CURB AND HEADER WITHIN AREA ENCLOSED BY HEAVY LINES INDICATES THE PAY LIMIT FOR VERTICAL CURB OR SLOPING CURB OF THE APPROPRIATE ADJACENT SIZE AND KIND.
 - WHERE THE DISTANCE FROM THE GUTTER LINE TO THE OUTSIDE EDGE OF SIDEWALK IS 6 FEET OR LESS, USE CURB RAMP TYPE 7, INSTEAD OF CURB RAMP TYPE 1 THROUGH 4.
 - CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED. SEE PLANS.
 - THE 12H:1V MAX SLOPE IS THE RUNNING SLOPE FOR CURB RAMPS, BUT ONLY THE 12H:1V SLOPE MEASURED AS X₂ IS THE RUNNING SLOPE FOR TYPE 3 AND TYPE 4 CURB RAMPS. ENSURE THE RUNNING SLOPE OF CURB RAMPS DOES NOT REQUIRE ITS LENGTH TO EXCEED 15 FEET. THE RUNNING SLOPE MAY EXCEED THE 12H:1V MAX SLOPE SO AS NOT TO EXCEED THE 15 FEET MAXIMUM LENGTH.
 - CURB RAMP TYPE 1 THROUGH 7 ARE NORMALLY PLACED ON THE RADIUS RETURN AT THE INTERSECTION AND ON A TANGENT SECTION AS DRAWN.



DETECTABLE WARNING SURFACE (CAST IRON)

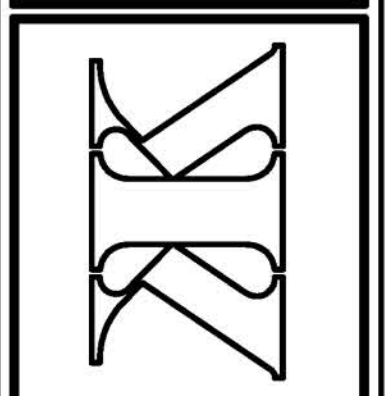


- DETECTABLE WARNING SURFACE NOTES:**
- KEEP TURNING SPACE, APPROACH SIDEWALK TRANSITIONS, AND CURB RAMP CLEAR OF OBSTRUCTIONS THAT PROTRUDE ABOVE THE SURFACE.
 - CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED, SEE PLANS.
 - FOR NARROW ISLAND WIDTH, SEE PEDESTRIAN REFUGE ISLAND WALKWAY OPENING AT INTERSECTIONS DETAIL.
 - FOR MEDIUM AND LARGE ISLAND WIDTH, SEE CURB RAMP TYPE 1 ON CD-606-1.
 - CONSTRUCT CURB RAMP TYPES 1, 2, 3, 4, & 7 PERPENDICULAR TO CURBLINE, AS SHOWN.
 - IF A CURB RAMP IS REQUIRED, THE LOCATION OF THE DETECTABLE WARNING SURFACE MUST BE AT THE BOTTOM OF THE RAMP AND WITHIN THE REQUIRED DISTANCE FROM THE RAIL.
 - A STANDARD DETECTABLE WARNING (DWS) SURFACE IS NOT AVAILABLE TO FIT THIS APPLICATION, AND THEREFORE ONE WILL NEED TO BE CUSTOMIZED. THE DWS SHOULD COVER THE ENTIRE WIDTH OF THE RAMP. THE ROWS OF DOMES ON THE DWS SHOULD FOLLOW THE DIRECTION OF TRAVEL OF THE RAMP, SO PEDESTRIANS WHO USE MOBILE DEVICES CAN TRACK BETWEEN THE DOMES.

6		6/13/18
5	NO REVISION THIS SHEET	5/16/18
4	NO REVISION THIS SHEET	3/28/18
3	NO REVISION THIS SHEET	10/17/17
2	PER SHPO COMMENT RE: CONCRETE CURB	
1	PER MDDOT COMMENT RE: ROW CERTIFICATION	

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 ANDREW CANGIANO, Professional Engineer, Lic. 43111



MAIN STREET SIDEWALK & STREETScape
 BLOOMFIELD AVENUE TO MINE STREET
CONSTRUCTION DETAILS
 BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
 FEDERAL AID PROJECT #TAP-1006(395)

GENERAL NOTES:

- ADVANCE WARNING SIGNS, DISTANCES, AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE ENGINEER, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
- THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
- PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE IN PLACE.
- RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN SHALL BE PROVIDED WITH AT LEAST ONE W20-IF SIGN (ROAD WORK AHEAD) AS A MINIMUM.
- ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS AND/OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN SHALL BE COVERED, REMOVED OR RELOCATED AS DIRECTED BY THE ENGINEER.
- CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY, OR PROPOSED TRAFFIC SIGNAL SYSTEMS SHALL BE BAGGED OR COVERED.
- MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES - PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS, AND SHALL BE APPROVED BY THE ENGINEER.
- CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) SHALL BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.
- A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH SHALL BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
- CONSTRUCTION SIGNS R11-4 (ROAD CLOSED TO THRU TRAFFIC) SHALL BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSTRUCTION.
- CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) SHALL BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST. THE PLACEMENT OF THESE SIGNS SHALL BE AS DIRECTED BY THE ENGINEER.
- MOVING WORK AREAS IN A PERMANENT LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER. THE TRUCK MOUNTED CRASH CUSHION SHALL MOVE WITH THE WORK AREAS TO KEEP A 60 FOOT MINIMUM AND 150 FOOT MAXIMUM BUFFER IN ADVANCE OF EACH WORK AREA.
- THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN SHALL BE SUBMITTED TO THE ENGINEER IN ACCORDANCE WITH SECTION 617 OF THE STANDARD SPECIFICATIONS.
- TRAFFIC SAFETY SERVICES SHALL BE USED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL, SECTION 617.
- ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY SHALL BE BACKFILLED AND PLACED ON A MINIMUM 6H : 1V SLOPE PRIOR TO THE END OF EACH WORK DAY. OTHER EXCAVATED AREAS WITHIN THE CLEAR ZONE ARE TO BE EITHER BACKFILLED OR A PRECAST CONCRETE CURB CONSTRUCTION BARRIER SET TEMPORARILY IN PLACE TO SHIELD VEHICULAR AND PEDESTRIAN TRAFFIC.
- WHERE REQUIRED, THE CONTRACTOR SHALL MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE ENGINEER.
- BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES SHALL BE TRANSITIONED ON A MINIMUM 20H : 1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
- THE PLACEMENT AND OR RELOCATION OF PRECAST CONCRETE CURB, CONSTRUCTION BARRIER SHALL BE DONE DURING APPROVED OFF-PEAK HOURS WHEN TRAFFIC MAY BE REDUCED TO ONE LANE IN EACH DIRECTION.
- CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY TRAFFIC SIGNAL & SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE RESIDENT ENGINEER.
- THE SPEED LIMIT, R2-1 (BLACK ON WHITE) SIGN SHALL BE LOCATED THROUGH WORK AREAS AS DIRECTED BY TRAFFIC SIGNAL & SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER - WORK ZONE.
- THE REDUCED SPEED AHEAD SIGN, R2-5A(S) (BLACK ON WHITE) SHALL BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
- TRAFFIC FINES DOUBLED IN WORK AREA R(N)S-17(S), 4 FEET BY 2.5 FEET SIGN SHALL BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN SHALL ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN SHALL BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
- THE FINAL BITUMINOUS CONCRETE SURFACE PAVEMENT SHALL NOT BE CONSTRUCTED UNTIL THE FINAL STAGE OF THE PROJECT. MANHOLES AND INLETS SHALL BE SET TO FINISHED GRADE AND TEMPORARY PAVEMENT RAMPS ARE TO BE CONSTRUCTED AROUND THEM WITH A MINIMUM 20H : 1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.

- TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. SHALL BE PLACED AS SHOWN ON PLANS. SIGNS SHALL NOT BE PLACED WITHOUT ACTUAL LANE CLOSURES AND SHALL BE IMMEDIATELY REMOVED UPON REMOVAL OF THE CLOSURES.
- CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE ENGINEER.
- TERMS
WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING SHALL BE AS FOLLOWS:
IMPACTS TO NORMAL TRAFFIC FLOW - WORK THAT REQUIRES A PORTION OF THE PAVED ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES, INCLUDING, BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC.
THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.
TEMPORARY LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.
PERMANENT LANE CLOSURES - WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.

27. ADVANCE NOTICES
FOR THE INITIAL START OF WORK THAT REQUIRES "Impacts to Normal Traffic Flow", THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IN WRITING, ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RESIDENT ENGINEER, THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESIDENT ENGINEER IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.

FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER IN WRITING, ON ADVANCE FORM TO-103, OF THE PROPOSED DATE. A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RESIDENT ENGINEER, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RESIDENT ENGINEER IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.

STARTING THE ESTABLISHMENT OF A NEW PERMANENT TRAFFIC PATTERN SHALL BEGIN NO EARLIER THAN 11:00 PM FRIDAY AND SHALL BE COMPLETED AND READY FOR OPERATIONS BY 6:00 PM THE FOLLOWING SUNDAY. THE ESTABLISHMENT SHALL BE COMPLETED IN ACCORDANCE WITH THE LANE CLOSURE HOURS SPECIFIED IN THE CONTRACT.

ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING SHALL BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE SPECIAL PROVISIONS.

28. PROGRESS NOTICES
ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY SHALL BE SUBMITTED TO THE RESIDENT ENGINEER BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-101 PROVIDED BY THE DEPARTMENT.

EACH DAY OF "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RESIDENT ENGINEER BY 9:00 AM THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-102 PROVIDED BY THE DEPARTMENT.

"TEMPORARY LANE CLOSURES" FOR WEEKENDS SHALL BE SUBMITTED TO THE RESIDENT ENGINEER BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-102 PROVIDED BY THE DEPARTMENT.

- CHANGES TO THE SCHEDULED CLOSURES
REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS SHALL BE SUBMITTED IN WRITING TO THE RESIDENT ENGINEER AS FOLLOWS:
CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.
OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" SHALL BE SUBMITTED TO THE RESIDENT ENGINEER AT LEAST FORTY CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.

LEGEND

- BREAKAWAY BARRICADES
- BREAKAWAY BARRICADES WITH SIGN
- CONSTRUCTION SIGNS
- DRUMS
- CONE
- PRECAST CONCRETE CURB CONSTRUCTION BARRIER (TYPE SPECIFIED)
- DIRECTION OF TRAFFIC FLOW
- FLAGGER
- ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING CAUTION MODE
- ILLUMINATED FLASHING ARROW MOUNTED ON TOWING VEHICLE SHOWING ARROW PATTERN (Left, Right, Both)
- TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING CAUTION MODE
- TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION AND ARROW BOARD SHOWING ARROW PATTERN (Left, Right, Both)
- TEMPORARY CRASH CUSHION, INERTIAL BARRIER SYSTEM
- TEMPORARY CRASH CUSHION, (all other approved)
- BUFFER ZONE
- WORK AREA
- PAINT STRIPING TRUCK OR OTHER OPERATING VEHICLE

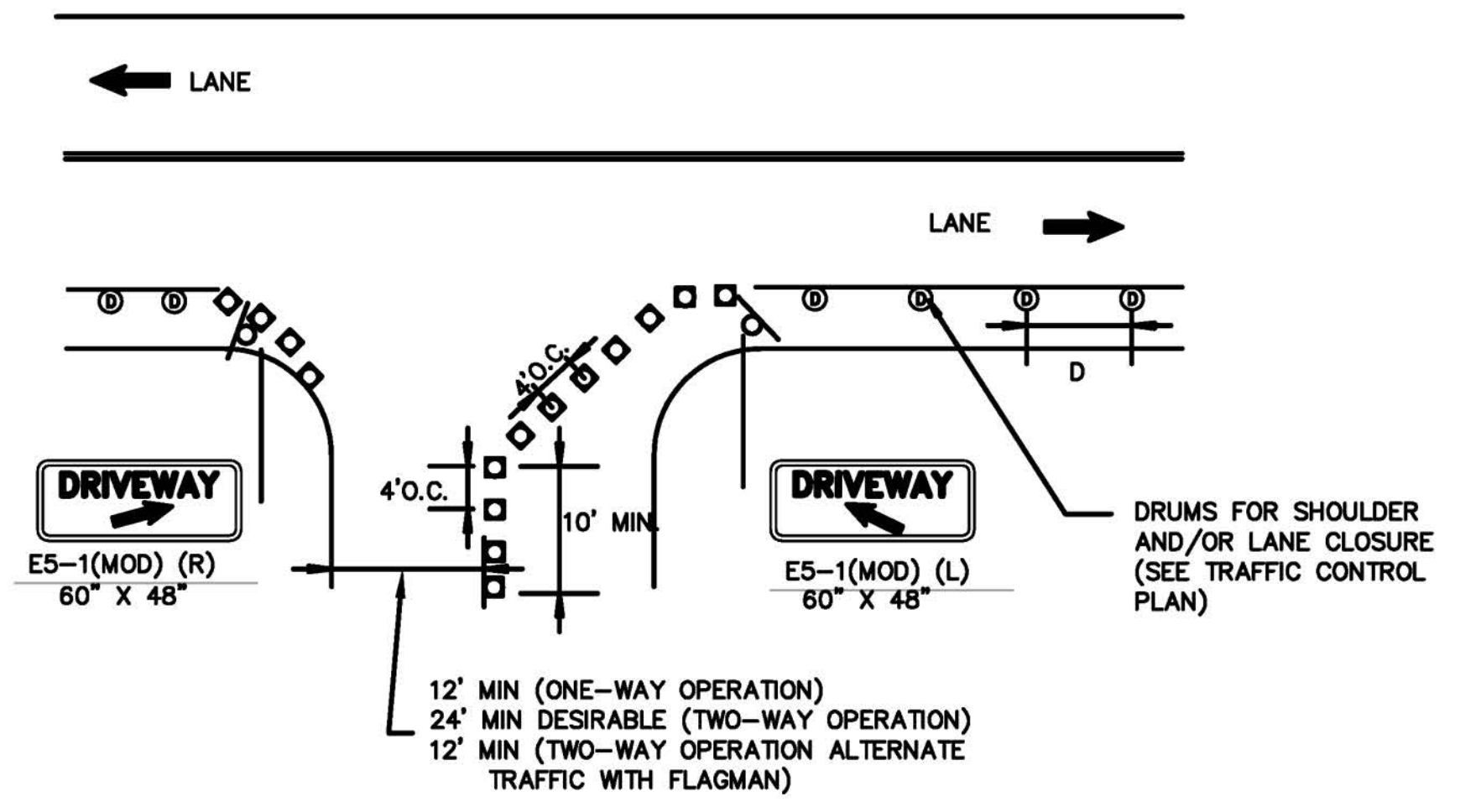
REGULATORY APPROACH SPEED OF TRAFFIC MILES/HOUR	RECOMMENDED SIGHT DISTANCE TO BEGINNING OF CHANNELIZING TAPERS		
	DESIRABLE		MINIMUM
	RURAL FEET	URBAN FEET	RURAL AND URBAN FEET
25	375	525	150
30	450	625	200
40	525	725	250
45	600	825	325
50	675	925	400
55	750	1025	475
60	825	1150	550
65	1000	1275	725

REGULATORY APPROACH SPEED OF TRAFFIC IN MILES/HOUR	RECOMMENDED TAPER LENGTH AND SPACING FOR CHANNELIZING TAPERS			RECOMMENDED SPACING ALONG TANGENTS	
	MINIMUM TAPER RATIO IN LENGTH PER FEET OF WIDTH	MINIMUM TAPER LENGTH L-FOR LANE WIDTHS	MAXIMUM DEVICE (B) SPACING ALONG TAPERS IN FEET	MAXIMUM DEVICE (D) SPACING ALONG TANGENTS IN FEET	
25	20:1	105	115	125	25
30	20:1	120	135	150	30
35	20.5:1	205	225	245	35
40	27.5:1	270	295	320	40
45	45:1	450	485	540	45
50	50:1	500	550	600	50
55	55:1	550	605	660	55

NOTE: THE MAXIMUM DEVICE SPACING ALONG CURVES SHALL BE AS DEFINED FOR TAPERS (B) IN THE ABOVE TABLE.

GENERAL NOTES FOR PLACEMENT OF TRAFFIC CONTROL DEVICES

- AVOIDANCE MANEUVER IS FOR A SPEED, PATH, AND/OR DIRECTION CHANGE PRIOR TO THE BEGINNING OF CHANNELIZING TAPERS.
- RECOMMENDED DISTANCES BETWEEN TWO SEPARATE LANE CLOSURES SHALL BE DOUBLE THE VALUES SHOWN ABOVE.
- RURAL AND URBAN ROAD DESIGNATIONS SHALL BE AS DEFINED IN THE NJDOT STATE HIGHWAY STRAIGHT LINE DIAGRAMS.
- DESIRABLE VALUES SHALL BE PROVIDED WHEREVER POSSIBLE. IF IT IS NOT FEASIBLE OR PRACTICAL TO PROVIDE DESIRABLE VALUES BECAUSE OF HORIZONTAL OR VERTICAL CURVATURE OR IF RELOCATION OF THE TAPER IS NOT POSSIBLE, THEN MINIMUM VALUES CAN BE APPLIED. WHEN MINIMUM VALUES ARE USED, SPECIAL ATTENTION SHOULD BE GIVEN TO THE USE OF SUITABLE TRAFFIC CONTROL DEVICES FOR PROVIDING ADVANCED WARNING OF THE CONDITIONS THAT ARE LIKELY TO BE ENCOUNTERED.
- TAPERS SHALL BE LOCATED TO MAXIMIZE THE VISIBILITY OF THEIR TOTAL LENGTH.
- NO CHANNELIZING TAPERS SHALL BE PLACED ACROSS AN INTERSECTING SIDE STREET. THE LOCATION OF CHANNELIZING TAPERS SHOULD BE ADJUSTED AS REQUIRED TO AVOID CROSSING A SIDE STREET.



- NOTES:
- CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS WITHIN WORK AREA AT ALL TIMES.
 - CONTRACTOR SHALL CONTACT PROPERTY OWNERS 3 DAYS IN ADVANCE OF WORKING WITHIN INDIVIDUAL DRIVEWAYS.

TRAFFIC CONTROL AT DRIVEWAYS DETAIL
N.T.S.

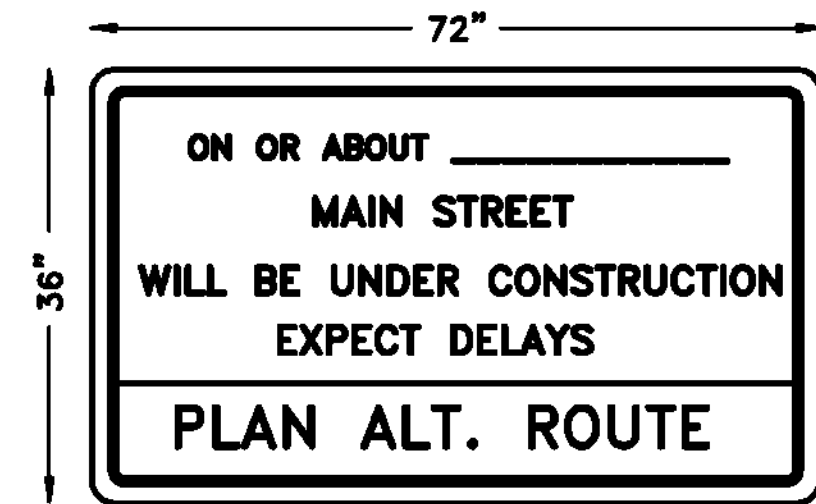
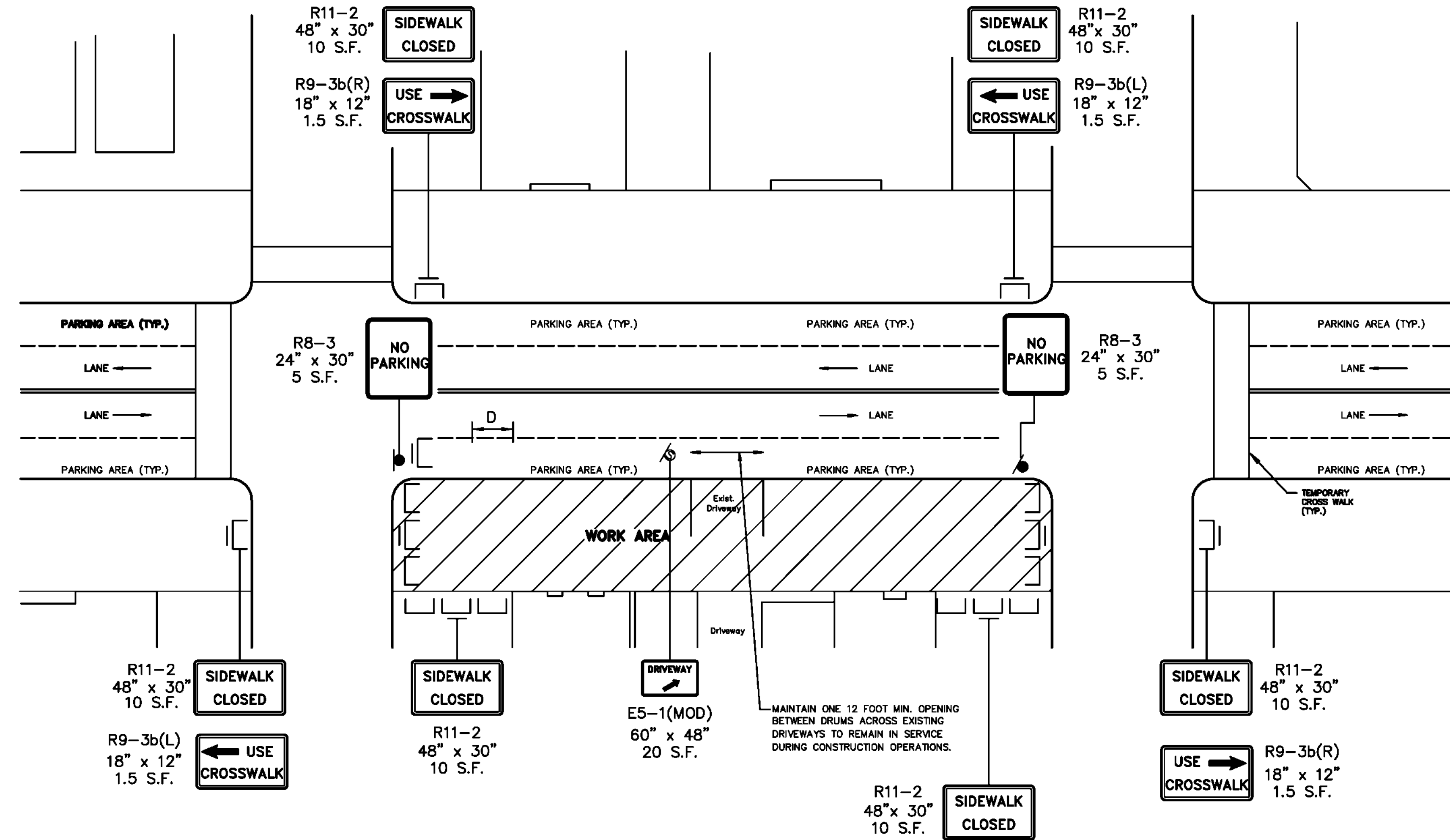
6	5	4	3	2	1
REVISIONS					
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NO REVISION THIS SHEET NO REVISION THIS SHEET PER SHPO COMMENT RE: CONCRETE CURB PER NJDOT COMMENT RE: ROW CERTIFICATION					
DATE: 4/24/17		SCALE: N.T.S.		F.L.D. BK.:	
PROJECT: 2016020		CHKD.:		PAGE:	
Keller & Kirkpatrick Inc. 500 Gibraltar Drive, Suite 204, Morris Plains, NJ 07960 973-377-8600 CONSULTING ENGINEERS LAND SURVEYORS LANDSCAPE ARCHITECTS PLANNERS ANDREW CANGIANO, Professional Engineer, Lic. 4311					
TRAFFIC CONTROL PLAN MAIN STREET SIDEWALK & STREETScape BLOOMFIELD AVENUE TO MINE STREET BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY FEDERAL AID PROJECT #TAP-1006(335)					
SHEET No. 29 OF 31					

GENERAL NOTES:

- DIMENSIONS, COLORS, AND DETAILS OF VARIOUS SIZE SIGNS, AND ACCESSORY PANELS TO FOLLOW STANDARDS IN THE CURRENT "STANDARD HIGHWAY SIGN PUBLICATION" AND THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS".
- (S) REPRESENTS A SPECIAL SIZE SIGN.
- LETTERS AND NUMERALS SHALL CONFORM TO THE CURRENT MANUAL, "STANDARDS ALPHABETS FOR HIGHWAY SIGNS" U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.
- THE CONTRACTOR SHALL OBTAIN THE APPROVAL OF THE ENGINEER FOR THE DISTANCE TO BE USED ON THE ADVANCE WARNING SIGNS, AND FOR THE SPEED LIMIT TO BE USED ON THE R2-1 SIGN.
- DISTANCE LEGEND: SIGN NUMBER FOLLOWED BY LETTER & DISTANCE

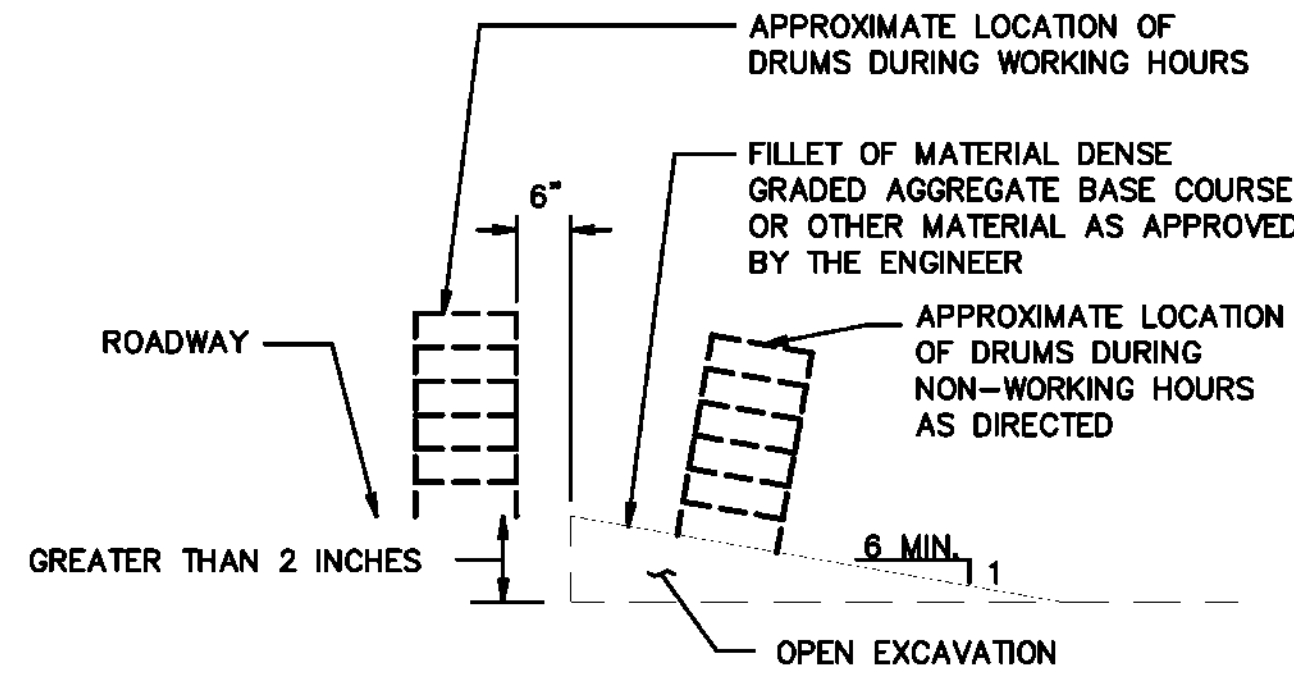
GENERAL NOTES FOR CONSTRUCTION WITHIN SIDEWALK AREAS

- THE TYPICAL WORK AREA DETAIL FOR SIDEWALK AND CURB CONSTRUCTION SHALL ALSO APPLY TO THE CONSTRUCTION OF ALL OTHER WORK WITHIN THE EXISTING SIDEWALK AREA.
- SIMULTANEOUS WORK ON ADJACENT BLOCKS IS PROHIBITED. CONTRACTOR SHALL COMPLETE ALL WORK, EXCLUDING MILLING & PAVING ASSOCIATED WITH FULL WIDTH RESURFACING ON ONE BLOCK BEFORE PROCEEDING WITH ANY WORK ON ADJACENT BLOCKS.
- ALL PAVEMENT RESTORATION REQUIRED FOR CONSTRUCTION OF NEW CONCRETE CURB SHALL BE CONSTRUCTED PRIOR TO OPENING ANY BLOCK/WORK AREA TO TRAFFIC, EXCEPT FOR THE FINAL TOP COURSE OF SUPERPAVE SURFACE COURSE, MIX 9.5M64, 1.5" THICK.
- MULTIPLE OPERATIONS MAY BE IN PROGRESS ON ONE SIDE OF THE STREET, BUT MUST BE SEPARATED BY AT LEAST TWO BLOCKS.
- CONTRACTOR SHALL LIMIT THE AMOUNT OF DISTURBED AREA TO UNDER 5000 SF AT ANY ONE TIME, AND ALL AREAS SHALL BE STABILIZED AND SECURED AT THE COMPLETION OF THE DAILY WORK.
- PLACEMENT OF DRUMS IN PARKING AREAS SHALL NOT ENCROACH EXISTING TRAVEL LANES DURING CONSTRUCTION ACTIVITIES ON ANY GIVEN BLOCK, EXCEPT FOR THE CONSTRUCTION OF DRAINAGE PIPE.
- FOR WORK ON ANY GIVEN BLOCK, CONSTRUCTION OF ANY UNDERGROUND WORK WITHIN THE EXISTING TRAVEL LANES SHALL BE STAGED SUCH THAT DISRUPTION OF THE TRAVEL LANES IS MINIMIZED. LANE CLOSURES SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE STANDARD TRAFFIC CONTROL DETAILS CONTAINED HEREIN.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL EXISTING PROPERTIES AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL DRIVEWAYS ON BELMONT AVENUE WITHIN THE PROJECT LIMITS DURING CONSTRUCTION.
- BREAKAWAY BARRICADES SHALL BE PLACED END TO END ACROSS THE ENTIRE WIDTH OF THE SIDEWALK AT THE LIMITS OF THE WORK AREA.
- THE CONTRACTOR SHALL PLACE "EMERGENCY-NO PARKING" SIGNS A MINIMUM OF 24 HOURS PRIOR TO CLOSING ANY BLOCK OR RESTRICTING ANY PARKING FOR ALL WORK AREAS.
- ALL WORK AREAS SHALL BE EITHER OPENED FOR PUBLIC ACCESS OR SECURED TO THE SATISFACTION OF THE RESIDENT ENGINEER DURING NON-WORKING HOURS.
- THE CONTRACTOR SHALL NOTIFY PROPERTY OWNERS, A MINIMUM OF 3 DAYS BEFORE COMMENCING WORK ON THAT BLOCK.



CONSTRUCTION ADVANCE WARNING SIGNS SHALL BE PLACED IF AND WHERE DIRECTED BY THE RESIDENT ENGINEER A MINIMUM OF 14 DAYS PRIOR TO COMMENCING WITH CONSTRUCTION.

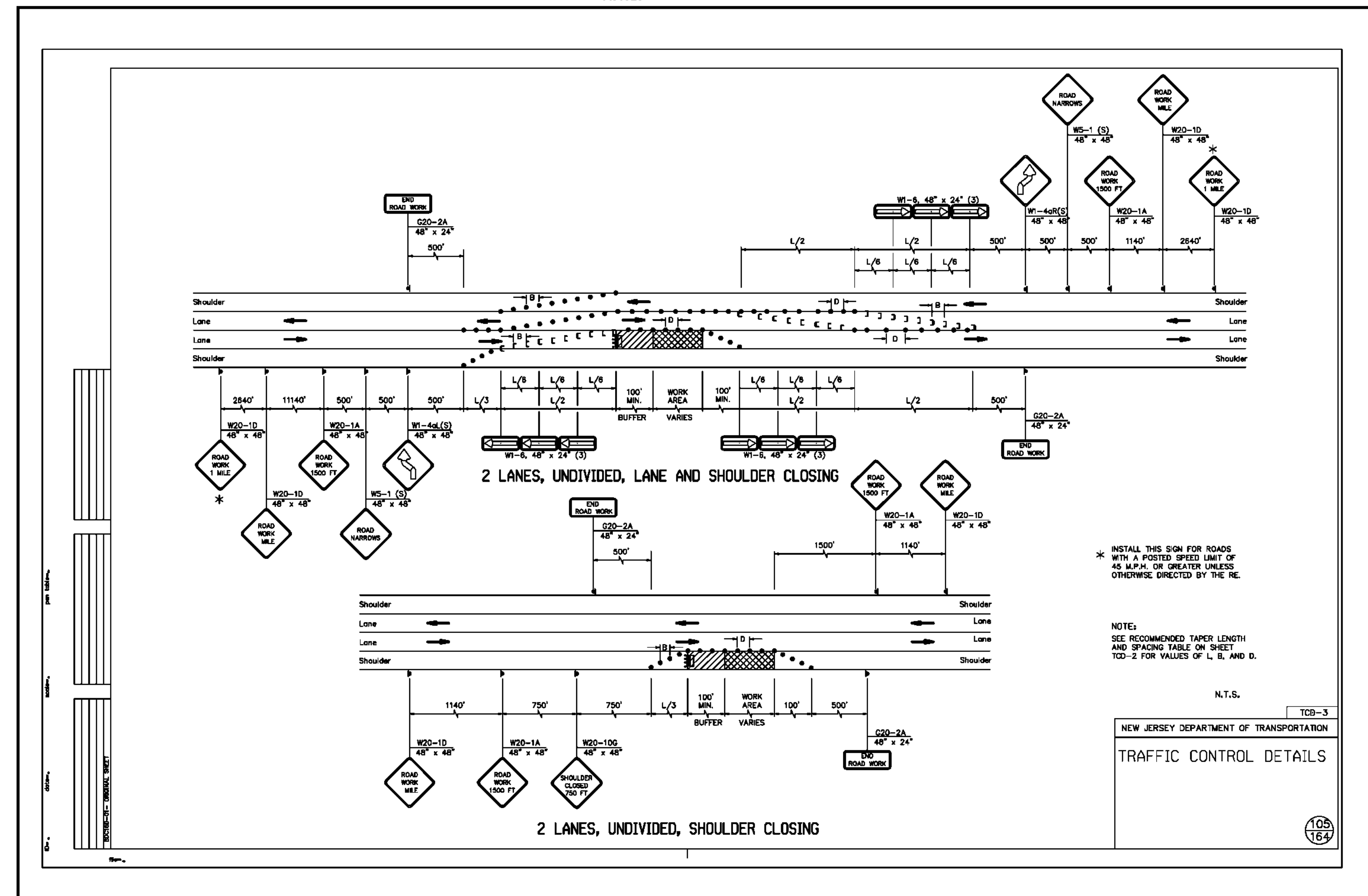
CONSTRUCTION IDENTIFICATION SIGN 'A'
N.T.S.



NOTE: ESCAPE RAMPS MUST BE CONSTRUCTED AND MAINTAINED DURING NON-WORKING HOURS WHERE A VERTICAL DROP OF GREATER THAN 2 IN EXISTS ADJACENT TO A TRAVELED LANE.

ESCAPE RAMP DETAIL
N.T.S.

TYPICAL WORK AREA DETAIL FOR SIDEWALK & CURB CONSTRUCTION
N.T.S.



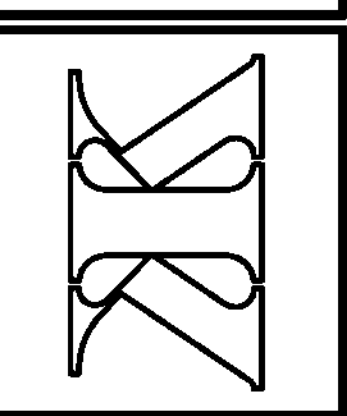
NOTE: SEE RECOMMENDED TAPER LENGTH AND SPACING TABLE ON SHEET TCD-2 FOR VALUES OF L, B, AND D.

N.T.S.
NEW JERSEY DEPARTMENT OF TRANSPORTATION
TRAFFIC CONTROL DETAILS

6	NO REVISION THIS SHEET	5/13/18
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Certificate of Authorization
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501 Gibraltar Drive, Suite 204, Morristown, NJ 07960 973-377-6800
LANDSCAPE ARCHITECTS
PLANNERS
ANDREY CANGIANO, Professional Engineer, Lic. 43111



TRAFFIC CONTROL PLAN
MAIN STREET SIDEWALK & STREETScape
BLOOMFIELD AVENUE TO MINE STREET
BOROUGH OF FLEMINGTON, HUNTERDON COUNTY, NEW JERSEY
FEDERAL AID PROJECT #TAP-1006(335)

SHEET No.
30
OF
31

SEE SHEET 2 OF 31 FOR GENERAL NOTES